

## REPORT FOR CONSIDERATION AT PLANNING SUB-COMMITTEE

### 1. APPLICATION DETAILS

**Reference No:** HGY/2015/2517

**Ward:** Highgate

**Address:** 191-201 Archway Road, London N6 5BN

**Proposal:** Erection of building behind retained Archway Road facade and fronting Causton Road to provide 25 residential dwellings (Class C3) at basement, ground, first, second and third floor level, including retention side return wall on Causton Road. Demolition of all existing buildings to the rear. Retention of retail floor space unit at ground floor level (Class A1). Change of use of part ground floor and part basement from retail (Class A1) to Class B1 use. Provision of associated residential amenity space, landscaping and car parking.

**Applicant:** Archway Apartments Ltd

**Ownership:** Private

**Case Officer Contact:** Aaron Lau

**Site Visit Date:** 08/06/2015

**Date received:** 12/05/2015 **Last amended date:** 21/10/2015

#### **Drawing number of plans and documents:**

- 499-0000-GA Rev 1 (Existing Site Location Plan)
- 499-0001-GA Rev 1 (Existing Site Plan)
- 499-0010-GA Rev 1 (Existing Ground Floor Plan)
- 499-0011-GA Rev 1 (Existing First Floor Plan)
- 499-0012-GA Rev 1 (Existing Second Floor Plan)
- 499-0013-GA Rev 1 (Existing Third Floor Plan)
- 499-0020-GA Rev 1 (Existing Basement Floor Plan)
- 499-0030-GA Rev 1 (Existing Section AA)
- 499-0031-GA Rev 1 (Existing Section BB)
- 499-0040-GA Rev 1 (Existing North East Elevation)
- 499-0041-GA Rev 1 (Existing North West Elevation)
- 499-0042-GA Rev 1 (Existing South East Elevation)
- 499-0043-GA Rev 1 (Existing South West Elevation)

- 499-0100-GA Rev 1 (Proposed Site Location Plan)
- 499-0110-GA Rev 1 (Demolition Ground Floor Plan)
- 499-0111-GA Rev 1 (Demolition First Floor Plan)
- 499-0112-GA Rev 1 (Demolition Second Floor Plan)
- 499-0113-GA Rev 1 (Demolition Third Floor Plan)
- 499-0120-GA Rev 1 (Demolition Basement Floor Plan)
- 499-0130-GA Rev 1 (Demolition Section AA)
- 499-0131-GA Rev 1 (Demolition Section BB)
- 499-0140-GA Rev 1 (Demolition North East Elevation)
- 499-0141-GA Rev 1 (Demolition North West Elevation)
- 499-0142-GA Rev 1 (Demolition South East Elevation)
- 499-0143-GA Rev 1 (Demolition South West Elevation)
- 499-0200-GA Rev 13 (Proposed Ground Floor Plan)
- 499-0201-GA Rev 7 (Proposed First Floor Plan)
- 499-0202-GA Rev 7 (Proposed Second Floor Plan)
- 499-0203-GA Rev 7 (Proposed Third Floor Plan)
- 499-0204-GA Rev 2 (Proposed Roof Plan)
- 499-0210-GA Rev 12 (Proposed Basement Plan)
- 499-0220-GA (Proposed Cycling Provision)
- 499-0300-GA Rev 2 (Proposed Section AA)
- 499-0301-GA Rev 2 (Proposed Section BB)
- 499-0302-GA Rev 2 (Proposed Section CC)
- 499-0303-GA Rev 2 (Proposed Section DD)
- 499-0304-GA Rev 2 (Proposed Section EE)
- 499-0400-GA Rev 3 (Proposed North East Elevation)
- 499-0401-GA Rev 3 (Proposed North West Elevation)
- 499-0402-GA Rev 3 (Proposed South West Elevation)
- 499-0403-GA Rev 3 (Proposed South East Elevation)
- Arboricultural Impact Assessment Report ref. PSP/191ACR/AIA/01a
- Daylight and Sunlight Assessment ref. A2500/DS/001
- Design and Access Statement dated August 2015
- Energy Statement & BREEAM Pre-assessment dated 4<sup>th</sup> June 2015
- Framework Travel Plan ref. MTP Ref: 15/025
- Heritage Statement dated August 2015
- Noise Assessment ref. A2500/N/002
- Planning Statement dated August 2015
- Transport Statement ref. MTP Ref: 15/025

**1.1** This application is being reported to Planning Committee as it is a major planning application and is required to be reported to committee under the current delegation.

## 1.2 SUMMARY OF KEY REASONS FOR RECOMMENDATION

- This planning application is for the redevelopment of the site at Nos. 191 to 201 Archway Road including the retention of existing facade fronting Archway Road and side return wall on Causton Road. Planning permission is also sought for the demolition of all existing B1/D1/D2 buildings to the rear.
- The proposal will seek to retain the retail floor space unit at ground floor level (Class A1), to change of use of part ground floor and part basement from retail (Class A1) to Class B1 use.
- 25 new residential dwellings will be created consisting of 6 x 1 bedroom units, 12 x 2 bedroom units and 7 x 3 bedroom units on the basement, first, second and third floors along with 7 parking spaces of which 3 will be disabled bays and associated landscaping.
- The proposed development is considered acceptable in principle in this instance as it would provide residential dwellings and additional family-sized housing generally whilst contributing to the Borough's housing targets as set out in Haringey's Local Plan and the London Plan.
- The loss of the existing low quality workshop units is acceptable as they will be replaced by higher quality employment generating provision in the form of flexible and affordable B1 workspace.
- The proposed density of 78 units per hectare and 238 habitable rooms per hectare is of an acceptable density for the site as it falls within the appropriate density range as set out in the London Plan for this part of the Borough.
- The proposed development would not cause any material loss of amenity of that currently enjoyed by existing and surrounding occupiers and residents of Causton Road and Archway Road in terms of outlook, enclosure, and loss of daylight/sunlight, overshadowing, loss of privacy or overlooking.
- The proposals involve extensions to the rear and side of Causton Road. Although the proposals will cause some visual harm to the character and appearance of the conservation area the harm is considered to be less than substantial. This harm has been given considerable weight by officers but it is outweighed by the significant heritage benefits of the scheme as a whole.
- The development makes provision for wheelchair accessible units and has been designed to meet Lifetime Homes standards, and provides an acceptable level of living accommodation and amenity space for occupiers of the new development.

- A number of conditions have been suggested should any consent be granted requesting details of the construction management plan and servicing of the new commercial unit to ensure it does not prejudice existing road and parking conditions, namely vehicular movements along Archway Road, Causton Road and the local road network generally and would not have an adverse impact on pedestrian safety.
- The proposal is subject to a S106 legal agreement to secure an off site affordable housing contribution, financial contributions for carbon offsetting and towards the amendment of the TMO, affordable B1 workspace, employment opportunities during construction, 'car free' development and considerate constructors scheme.

## **2. RECOMMENDATION**

- 2.1 That the Committee resolve to GRANT planning permission and that the Head of Development Management is authorised to issue the planning permission and impose the conditions and informatives set out below subject to the signing of a section 106 Legal Agreement providing for the obligations set out in the Heads of Terms below.
- 2.2 That the section 106 legal agreement referred to in resolution 2.1 above is to be completed no later than 31<sup>st</sup> November 2015 or within such extended time as the Head of Development Management or the Assistant Director Planning shall in her/his sole discretion allow; and
- 2.3 That, following completion of the agreement referred to in resolution 2.1 above within the time period provided for in resolution 2.2 above, planning permission be granted in accordance with the Planning Application subject to the attachment of the conditions and informatives set out below.

### **Conditions:**

- 1) Development begun no later than three years from date of decision
- 2) In accordance with approved plans
- 3) Materials
- 4) Landscaping
- 5) Shopfront
- 6) A1 hours of opening
- 7) B1 hours of opening
- 8) NOx boilers
- 9) Community heat boilers
- 10) Air Quality and Dust Management Plan
- 11) Considerate Constructors Scheme
- 12) Demolition and construction plant and machinery
- 13) NRMM
- 14) Heat network

- 15) BREEAM
- 16) Drainage strategy
- 17) Design and method statements
- 18) Archaeological programme
- 19) Construction Management Plan / Construction Logistics Plan
- 20) Delivery and Servicing Plan
- 21) Section 278
- 22) Residential cycle parking
- 23) Commercial cycle parking
- 24) Car parking accommodation

**Informatives:**

- 1) Co-operation with the applicant
- 2) CIL liable
- 3) Hours of construction
- 4) Party Wall Act
- 5) Signage
- 6) Advertisement
- 7) Street numbering
- 8) Thames Water
- 9) London Underground
- 10) Historic England
- 11) Waste
- 12) London Fire Brigade
- 13) Asbestos

**Section 106 Heads of Terms:**

- 1) £255,000 towards affordable housing.
- 2) £1,000 towards the amendment of the TMO to secure the 'car free' development, and two years free membership to a local Car Club and £50 free credit per unit.
- 3) £3,291 towards short-stay cycle parking on the adjoining public realm.
- 4) £22,410 to the Council's carbon offsetting fund.
- 5) Affordable B1 workspace – capping rents.
- 6) Participation in the Council's employment initiatives during construction phase.
- 7) Considerate constructors' scheme.

2.4 In the event that members choose to make a decision contrary to officers' recommendation members will need to state their reasons.

2.5 That, in the absence of the agreement referred to in resolution 2.1 above being completed within the time period provided for in resolution 2.2 above, the planning permission be refused for the following reasons:

- (i) In the absence of a financial contribution towards Affordable Housing, the proposal would have an unacceptable impact on affordable housing provision within the Borough. As such, the proposal would be contrary to Local Plan policy SP2 and London Plan policy 3.12.
  - (ii) In the absence of a financial contribution towards the amendment of the Traffic Management Order and short-stay cycle parking, the proposal would have an unacceptable impact on the highway and fail to provide a sustainable mode of travel. As such, the proposal would be contrary to Local Plan policy SP7, saved UDP policy UD3 and London Plan policies 6.9, 6.11 and 6.13.
  - (iii) In the absence of a financial contribution towards the carbon offsetting, the proposal would fail to deliver an acceptable level of carbon saving. As such, the proposal would be contrary to Local Plan policy SP4 and London Plan policy 5.2.
- 2.6 In the event that the Planning Application is refused for the reasons set out in resolution 2.5 above, the Head of Development Management (in consultation with the Chair of Planning sub-committee) is hereby authorised to approve any further application for planning permission which duplicates the Planning Application provided that:
- (i) There has not been any material change in circumstances in the relevant planning considerations, and
  - (ii) The further application for planning permission is submitted to and approved by the Assistant Director within a period of not more than 12 months from the date of the said refusal, and
  - (iii) The relevant parties shall have previously entered into the agreement contemplated in resolution 2.1 above to secure the obligations specified therein.

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### 3.0 PROPOSED DEVELOPMENT AND LOCATION DETAILS

#### 3.1 Proposed development

- This planning application is for the redevelopment of the site at Nos. 191 to 201 Archway Road including the retention of existing facade fronting Archway Road and side return wall on Causton Road. Planning permission is also sought for the demolition of all existing B1/D1/D2 buildings to the rear.
- The proposal will seek to retain the retail floor space unit at ground floor level (Class A1), to change of use of part ground floor and part basement from retail (Class A1) to Class B1 use.
- 25 new residential dwellings will be created consisting of 6 x 1 bedroom units, 12 x 2 bedroom units and 7 x 3 bedroom units on the basement, first, second and third floors along with 7 parking spaces of which 3 will be disabled bays and associated landscaping.

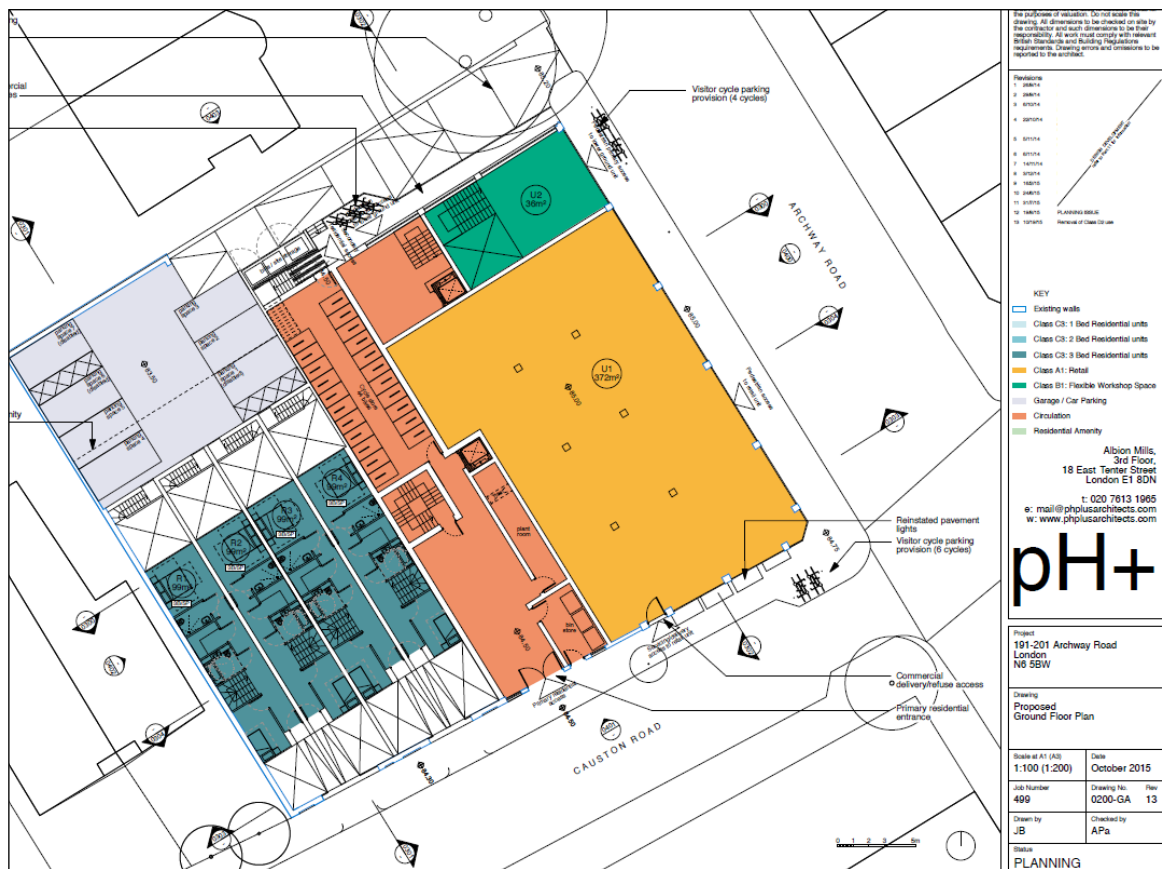
<b>No. of bedrooms</b>	<b>No. of units</b>	<b>% of units</b>
1 bed units	6	24
2 bed units	12	48
3 bed units	7	28
<b>TOTAL</b>	<b>25</b>	<b>100</b>

- A flexible Class B1/D2 use was proposed for the lower ground floor as part of the original planning application submission, but following comments raised during the extensive public consultation, and discussions with Officers, the applicant has revised the scheme to include only Class B1 use only This B1 space is also to be affordable and flexible B1 workspace ad is secured as such by a section 106 legal agreement.



## 3.2 Site and Surroundings

3.2.1 The application site at Nos. 191 to 201 Archway Road is broadly square in shape and currently comprises a three-storey building with front gable ends and a single-storey front projection located on the corner of Archway Road and Causton Road.



Proposed ground floor plan

3.2.2 The main building fronting onto Archway Road is occupied by a retail unit (Use Class A1) known as Richardsons of Highgate furniture shop in the basement, ground and first floors. It is understood that the shop ceased retail operations in December 2014, but part of the unit has continued to trade on an ad-hoc basis as a furniture shop since its closure.

3.2.3 The rest of the basement floor and the rear of the site are currently occupied by an assortment of different B1/D1/D2 units namely:

- Unit 1 (ground floor): Furniture repair with interior design office (Use Class B1c / B1a)
- Unit 2 (basement): Cycle repairs and sales (Use Class B1c)
- Unit 3 (basement): Cycle repairs and sales (Use Class B1c)

- Unit 4 (basement): Counselling service (Use Class D1)
- Unit 5 (basement): Furniture upholsterer (Use Class B1c)
- Unit 6 (basement): Painting studio (Use Class B1c)
- Unit 7 (basement): Yoga studio (Use Class D2)
- Unit 8 (basement): Vacant (previously occupied by a furniture upholsterer) (Use Class B1c)
- Unit 9 (basement): Personal Training fitness studio (Use Class D2)
- Units 10-11 (basement): Cabinet maker (Use Class B1c) – basement
- Unit 12 (basement): TV editing / post production (Use Class B1c)
- Unit 13 (basement): Office (Use Class B1a)
- Units 14 (ground floor): Leather cutting (Use Class B1c)



3.2.4 For the avoidance of doubt: Use Class B1a are offices other than use within Class A2 (Financial and Professional Services; Use Class B1(c) are for any industrial process which can be carried out in a residential area without causing detriment to amenity; Use Class D1 are non-residential intuitions; and Use Class D2 are assembly and leisure establishments.

3.2.5 Further to the mixed units located on the basement and ground floors, the upper floors on the site are currently occupied by 4 separate Houses in Multiple Occupation (HMOs) - Use Class C4 – small HMO. (A small HMO is described as a dwelling occupied by between three and six unrelated individuals as their only or main residence).

3.2.5 The application site is located in the Highgate Conservation Area as designated in the Local Plan Proposals Map. Archway Road Local Shopping Centre is located opposite and on the eastern side of Archway Road.

3.2.6 There is protected Metropolitan Open Land (MOL) and a Site of Importance and Nature Conservation (SINC) situated some 100m west of the site.

3.2.7 The site has no designation in the current Site Allocations DPD Regulation 19 Consultation Document, approved by Cabinet on 20<sup>th</sup> October 2015, or the draft neighbourhood plan.

### 3.3 Relevant Planning history

- OLD/1961/0012 - Illuminated overhanging sign on business premises. – approved 05/10/1961
- OLD/1954/0014 - Provision of iron staircase & balcony. – approved 22/12/1954
- OLD/1954/0013 - Addition at rear providing new bathroom & WC. – approved 24/02/1954
- OLD/1952/0011 - Conversion of 3rd floor storeroom into self-contained flat. – approved 21/11/1952

## 4. CONSULTATION RESPONSE

4.1 **Planning Committee Pre-application:** the proposal was presented to the 28 July 2014 pre-application briefing meeting of the planning committee.

4.1.1 The notes of the meeting are set out as follows:

- *The applicant's early intentions in relation to affordable housing provision were questioned within the context of the Council's 50% target. The applicant advised that consultants had been engaged to undertake a viability assessment but that initial proposals were for a tenure blind development with affordable units provided onsite, potentially layered at first floor level.*
- *In response to concerns regarding the loss of the current employment space onsite, it was advised that the space was of low quality and hence suffered from low occupancy rates.*
- *The demand for additional A1 units on Archway Road was questioned. The applicant advised that discussions were progressing with a number of interested businesses in the retail/leisure sector.*
- *Clarification was given on the intention to provide 7 parking spaces onsite allocated to the larger residential units and wheelchair accessible unit, with the remainder of the site designated car free.*
- *The Committee requested that consideration be given to design features to make the front fascia less prominent and the use of the space in front of the bay windows as an accessible green roof space.*

- *Confirmation was provided that the amenity space provided would exceed the minimum standards required.*
- *A request from the Highgate Neighbourhood Forum for the retention of the workshop space currently provided onsite was passed onto the applicant.*
- *The applicant provided assurance that external metal roller shutters would not be used to the ground floor commercial units.*
- *The Committee queried the future management of deliveries to the proposed commercial units. The applicant advised that a management plan was currently being developed, with likely continuation of current loading arrangements off Causton Road, with accompanying restrictions on hours of use.*

4.2 The scheme was presented to the **Haringey Quality Review Panel** on 20<sup>th</sup> May 2015.

4.2.1 A summary of their response is set out as follows:

*The panel broadly supports the development proposals for 191 – 201 Archway Road, which promise restoration of the existing 19th century façade, with high quality contemporary development behind. There remains scope for further refinement in the architectural expression of new elements of the scheme, including the rebuilt shop front. The density of development proposed also creates challenges in achieving high quality residential accommodation. The panel thinks that introduction of workspace could help address this, as well as adding to the vitality of the area. More detailed comments are provided below on: the commercial unit; Archway Road block; courtyard block; and mix of uses.*

The design has been amended following the panel review.

4.3 **Haringey Development Management Forum** was held on 18 May 2015

4.3.1 The notes are set out as follows:

- *Residents made the following comments on the scheme following a short presentation by the developer's team:*
  - *Concerns were raised with regard to the loss of the existing small community studios/workshops located at the rear. The applicant explained a majority are vacant and of low quality and will be discussing its loss with the Council.*

- *The new commercial unit would have an impact on local businesses and does not benefit local traders. It was further noted that the draft Neighbourhood Plan has a policy to retain small businesses on-site.*
- *Cllr Carter emphasised the importance to retain the design and colour of the existing shopfront. The applicant provided assurance that the ivory colour will be retained.*
- *The servicing and operation of the new commercial unit was raised as a concern. The applicants explained that no servicing can take place on Archway Road (red route) and a Transport Assessment/Travel Plan can capture the necessary details.*
- *Some residents supported the idea of the gym proposal.*
- *Existing occupiers wanted to know whether assistance can be provided in terms of relocation. The applicants agreed to support their relocation.*
- *The parking provision was queried given the existing parking problems. It was explained that the parking will be allocated to families/disabled people and the remaining occupiers will not have access to parking.*
- *The location of the refuse was questioned as there is currently a vermin problem on adjacent sites. The applicant provided an explanation that the waste will be collected by independent collectors in agreement with the Council.*

4.4 The following were consulted regarding the application:

- LBH Housing Design & Major Projects
- LBH Housing Renewal Service Manager
- LBH Arb
- LBH EHS - Noise & Pollution
- LBH Cleansing
- LBH Conservation Officer
- LBH Economic Development
- LBH Building Control
- LBH EHS - Contaminated Land
- LBH Transportation
- LBH Carbon Management
- LBH Drainage
- The Highgate Society
- Highgate CAAC
- London Fire Brigade
- Designing Out Crime Officer

- Arriva London
- (R) Cholmeley & Causton Residents Association
- (R) Archway Road Residents Association
- Transport For London (TfL)
- Environment Agency
- London Underground
- Thames Water
- Greater London Archaeology Advisory Service (GLAAS)
- Historic England

The following responses were received:

Internal:

- 1) LBH Conservation Officer: No objection subject to a materials condition.

*“As per the Council’s statutory duty and in context of the Barnwell Manor case, it is considered that the proposed scale of the extensions to the existing building would cause some harm. This harm has been given great weight in assessing whether the new development preserves or enhances the conservation area. It is considered that the new development is of a high quality and would preserve the significant façades of the building, whilst securing a long term use of the building for a sustainable future. As such the proposal would preserve and enhance the conservation area and the limited harm caused by the scale of the rear extension would be outweighed by the significant public and heritage benefits. The proposal is, therefore, acceptable from a conservation point of view.”*

- 2) LBH Transportation: No objection subject to a S106 agreement securing a car-free development including a financial contribution of £1,000 towards the amendment of the Traffic Management Order, 2 years free membership to a local Car Club and £50 free credit, £3,291 towards commercial cycle parking and conditions covering construction management plan, S278 highway works, delivery and servicing plan, parking and cycling.
- 3) LBH Carbon Management: No objection subject to a financial contribution of £22,410 to the Councils carbon offsetting fund and heat network and BREEAM conditions.
- 4) LBH Environmental Health: No objection subject to NOx boilers, community heat boiler, management plan, considerate constructors scheme, demolition and NRMM conditions
- 5) LBH Cleansing: No objection subject to informatives.

External:

- 6) Thames Water: No objection subject to a drainage strategy condition and an informative.
- 7) London Fire Brigade: No objection subject to an informative.
- 8) London Underground: No objection subject to a design and method condition.
- 9) Environment Agency: No comments.
- 10) Historic England: No objection subject to an archaeological condition.

## 5. LOCAL REPRESENTATIONS

5.1 The following were consulted:

- 690 Neighbouring properties
- Residents Associations (The Highgate Society, Highgate CAAC, Cholmeley & Causton Residents Association & Archway Road Residents Association)
- 1 site notice was erected close to the site
- 1 press notice dated 11<sup>th</sup> September 2015

5.2 The number of representations received from neighbours, local groups etc in response to notification and publicity of the application were as follows:

No of individual responses: 26

Objecting: 25

Supporting: 1

Petitions against the proposal containing 223 signatures

5.3 The following local groups/societies made representations:

- The Highgate Society;
- Highgate CAAC (In support); and
- Cromwell Area Resident's Association (CARA)

5.4 The following MP made representations:

- Catherine West MP

5.5 The following issues were raised in representations that are material to the determination of the application and are addressed in the next section of this report:

- Overdevelopment;
- Loss and displacement of existing independent businesses and traders including loss of jobs and services;

- Contrary to Haringey's Sustainable Community Strategy that seek to 'ensure economic vitality and prosperity is shared by all, through promoting a vibrant economy , increasing skills, raising employment and reducing worklessness';
- Impact on existing local and independent shops;
- Highway and pedestrian safety from the servicing of the site;
- Design, scale and bulk of the proposal;
- Impact on conservation area;
- Loss of privacy;
- Loss of daylight/sunlight and overshadowing;
- Increased parking pressures on the surrounding roads;
- Noise pollution from service deliveries;
- Disturbance caused by construction vehicles (Officer comments: details will be sought under a CMP condition);
- Lack of affordable housing;
- Flood risk
- No clear public benefits

5.6 The following issues raised are not material planning considerations:

- Impact on the foundations of adjacent buildings;

## **6 MATERIAL PLANNING CONSIDERATIONS**

6.1 The main planning issues raised by the proposed development are:

1. Principle of the development
2. Siting, Layout and Design
3. Impact of the proposed development on the character and appearance of the conservation area
4. Housing
5. The impact on the amenity of adjoining occupiers
6. Living conditions for future occupants
7. Parking and highway safety
8. Accessibility
9. Trees
10. Sustainability
11. Flood Risk

### **6.2 Principle of the development**



### *Residential use*

- 6.2.1 Local Plan Policy SP1 sets out the strategic vision to provide up to 5,000 new homes by 2026, which aligns with the aspirations of Policy SP2, which has a current target of providing 1,502 new homes a year in Haringey between the period 2015 to 2025 under The London Plan (FALP) 2015. The provision of housing would in principle be supported as it would augment the Borough's housing stock in accordance with Local Plan Policies SP1 and SP2, and London Plan Policy 3.3.
- 6.2.2 The proposed number of residential units on the site comprising 6 x 1 bedroom units, 12 x 2 bedroom units and 7 x 3 bedroom units will contribute to providing housing to assist in meeting this housing target.

### *Loss of existing employment occupiers*

- 6.2.3 Residents and amenity groups have expressed significant concerns over the loss and displacement of existing independent businesses and traders.
- 6.2.4 The loss of the existing B1 floor space is a fundamental planning consideration and Local Plan Policy SP8 makes it clear that there is a presumption to support local employment and small sized businesses that require employment land and space. It is also important to note that draft DPD Policy DM40 (B) states that the Council will only consider the loss of employment land or floorspace is acceptable, subject the new development proposals provide the maximum amount of replacement employment floorspace possible, as determined having regard to viability. Although only limited weight can be afforded to draft DPD DM policies given its current status which is early in the adoption process.
- 6.2.5 Furthermore saved UDP Policy EMP4 encourages the redevelopment of unallocated employment sites providing that: the land or building is no longer suitable for business or industry use on environmental, amenity and transport grounds in the short, medium and long term; and the redevelopment or re-use of all employment generating land and premises would retain or increase the number of jobs permanently provided on the site, and result in wider regeneration benefits.
- 6.2.6 The applicant has confirmed that approximately 15 jobs are provided by the existing A1 and B1 uses currently on site. The majority of the employed people occupy the small workshop-style B1(c) units situated to the rear of the site. These units are in very poor condition and rents are therefore extremely low to reflect this. It is understood the quality of the accommodation has been in this condition for some time. However it is not considered that the land is no longer suitable for employment use.

- 6.2.7 Draft Policy DM38 (C) (IV) requires a proportion of the employment floorspace is provided as 'affordable workspace' where viable. Officers consider £12 per square foot is considered to be 'affordable' in terms of employment generating workspace in the form of flexible start up units (Class B1) in the Borough, whereas the applicant has adopted an £8 per square foot in their appraisal which has been independently verified by the Council. At the same, the rental value of the B1 workspace offered by the applicant would fall in line with the Borough's definition of workspace being 'affordable' and thus would meet the policy requirements of draft DPD Policy DM38 (C) (iv).
- 6.2.8 The existing 697sqm B1 floorspace will be replaced by 707sqm of affordable B1 workspace; therefore there will be no net loss of B1 floorspace. In terms of employment generation for the new A1/B1 units to be provided and using the methodology set out in The Homes and Community Agency's Employment Densities Guide (2010) – it is estimated that the proposal will provide a total of 59 full-time jobs on the site (15 employees for the A1 floorspace circ. 377sqm, and 44 employees for the B1(a) floorspace circ. 707sqm). This represents a net increase of 44 jobs, and as such the proposal will provide a clear uplift in the number of potential jobs and a higher quality of employment space including affordable workspace in meeting the requirements of saved UDP Policy EMP4, Local Plan Policy SP8 and draft DPD Policy DM50.

*Loss of HMO units and D1 counselling office / New B1 use*

- 6.2.9 The upper floors on the site are currently occupied by 4 HMOs (Use Class C4). Saved UDP Policy HSG6 provides guidance for a change of use from an HMO to a single dwelling house. The change of use will only be considered: where the property is small and only 2 storeys; where the property does not meet the appropriate standards and has no realistic prospect of meeting the standards; or where the property is in a Housing Renewal Area and is not registered.
- 6.2.10 Draft DPD Policy DM17 further states that the Council will allow for the possibility of returning converted properties to single family dwellings.
- 6.2.11 It should be noted however that the loss of the HMO units could be secured under permitted development in line with The General Permitted Development Order 1995 (as amended) which allows for a permitted change of use from Class C4 HMO accommodation to Class C3 - residential and without the need to apply for planning permission.
- 6.2.12 A survey of the site reveals the existing HMOs on site are of a poor quality. This is consistent to the supporting text to saved UDP Policy HSG6 which identifies many HMO in Haringey are sub standard and the Council aims to ensure that standards are improved to provide satisfactory living conditions or where this is not possible encourage the buildings to be converted back to single dwelling houses. As such, the loss of the existing HMOs to facilitate the provision of 25

residential units on the site will be acceptable in principle as it would provide an uplift in both the quality and quantity of accommodation.

6.2.13 Elsewhere, there is currently a 12sqm counselling office (Class D1) located in the basement and to the rear of the site. Planning records show this D1 unit does not have the benefit of planning permission and has been established over time. The office is in a poor condition. Although Local Plan Policy SP16 seeks the protection of such community uses, its loss is significantly outweighed by the clear and wider benefits of the scheme such as the provision of higher quality employment space and residential accommodation. The loss of the D1 unit is therefore acceptable in this regard.

6.2.14 Part of the proposals is for new B1 floorspace of approximately 707 square metres in area replacing the basement floorspace associated with the existing furniture shop. Class B uses such as light industrial, logistics, warehousing and storage facilities are encouraged and sought to be protected by Local Plan Policy SP8. This is in response for the need to support small and medium sized businesses that require employment land and space. The reduction in trading floorspace afforded to the existing A1 use to facilitate a new B1 floor space would therefore be supported by Officers as it is considered a better quality of employment space which at the same time provides an active frontage at ground floor level fronting Archway Road.

*New A1 commercial unit*

6.2.15 The gross trading floorspace of the existing retail unit will be reduced from 917 sqm to 377sqm to provide a new ground floor commercial unit (this is likely to be let to a food retailer). The application site does not lie within a designated town centre, but Archway Road Local Shopping Centre, which is designated in the Unitary Development Plan Proposals Map, is located opposite and on the eastern side of Archway Road. Therefore the site is considered to be an 'edge of centre' site.

6.2.16 The need to protect local shopping facilities and services is outlined in Local Plan Policy SP10 and saved UDP Policy TCR4. The existing shop ceased retail operations in December 2014, but part of the unit has continued to trade on an ad-hoc basis as a furniture shop since its closure. The shop has not been renovated for a number of years and is under-utilised and of a low quality. In contrast, Officers consider the new commercial unit would significantly improve the quality of the retail floorspace on site which in turn enhance the vitality and viability of this commercial section of Archway Road in meeting the retail aims and objectives of the NPPF and Policy SP10 of the Local Plan, Policies 2.15, 4.7 and 4.8 of the London Plan and saved Policy TCR4 of the UDP. Given that the proposal replaces current retail floorspace this is considered to be acceptable subject to other detailed considerations.

### **6.3 Siting, Layout and Design**

- 6.3.1 Chapter 7 of the NPPF and London Plan Policies 7.4 and 7.6 require development proposals to be of the highest design quality and have appropriate regard to local context. Local Plan Policy SP11 and saved UDP Policy UD3 reinforce this strategic approach. The application site is located in the Highgate Conservation Area and is therefore subject to relevant conservation policies as set out within London Plan Policy 7.8, Haringey Local Plan Policy SP12 and saved Haringey UDP Policy CSV5
- 6.3.2 The proposal involves the demolition of the workshops to the rear. Additionally the single storey element to the north would also be demolished to allow for the residential accommodation to be extended. Given their utilitarian appearance and very limited contribution to the conservation area, Officers are supportive to the demolition proposed.
- 6.3.3 Part of the proposal is for the retention of the front and flank elevations of the building, with internal demolition with new flats proposed within the existing retained shell. In addition, the proposed scheme would repair the fabric on the front elevation and install more suitable windows on the first floor which is welcomed by Officers.
- 6.3.4 The scheme proposes to retain and rebuild the shop front at ground floor level incorporating the key design features of the original shop front and the shop front design principles included in the Highgate Conservation Area Management Plan. As such, Officers take the view that the shop front proposals would preserve as well as enhance the conservation area in terms of the commercial element of the building subject to the imposition of a signage conditions on any grant of planning permission.
- 6.3.5 The bulk of the development is to the rear and the flank where the gables would be extended to the rear with a small flat section in between the gables. Along Causton Road, the flank elevation is extended in a contemporary interpretation of the existing elevation. It also incorporates additional gables at the end. Overall the design, bulk and scale of the new development is acceptable as it would considerably enhance the appearance of the building and hence its contribution to the character and appearance of the conservation area as whole

#### *Density*

- 6.3.6 The density of a proposed development is relevant to whether the amount of development proposed is appropriate for a site. This is also dependent on the sites location and accessibility to local transport services. Local Plan Policy SP2 states that new residential development proposals should meet the density levels in the Density Matrix of the London Plan. Furthermore, objections have been

received from local residents that the proposal by virtue of the number of residential units offered would represent a gross overdevelopment on the site.

- 6.3.7 The density proposed of 78 (25 units / 0.32 Ha) units per hectare and 238 (76/0.32) habitable rooms per hectare accords with the guidelines set out in table 3.2 within London Plan Policy 3.4, which suggests a density of up to 260 u/ha and 700 hr/ha at this urban location (PTAL 4). Therefore, it is considered that the scheme does not constitute an overdevelopment on the site and the quantum of units proposed is acceptable in its local setting, subject to all other material planning considerations being met.

#### **6.4 Impact of the proposed development on the character and appearance of the conservation area**

##### *Statutory test*

- 6.4.1 Section 72(1) of the Listed Buildings Act 1990 provide:

- 6.4.2 *“In the exercise, with respect to any buildings or other land in a conservation area, of any functions under or by virtue of any of the provisions mentioned in subsection (2), special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.”* Among the provisions referred to in subsection (2) are *“the planning Acts”*.

- 6.4.3 The Barnwell Manor Wind Farm Energy Limited v East Northamptonshire District Council case tells us that *“Parliament in enacting section 66(1) did intend that the desirability of preserving listed buildings should not simply be given careful consideration by the decision-maker for the purpose of deciding whether there would be some harm, but should be given “considerable importance and weight” when the decision-maker carries out the balancing exercise.”*

- 6.4.4 The case of the Queen (on the application of The Forge Field Society) v Sevenoaks District Council sets out that the duties in Sections 66 and 72 of the Listed Buildings Act do not allow a Local Planning Authority to treat the desirability of preserving of listed buildings and the character and appearance of conservation areas as mere material considerations to which it can simply attach such weight as it sees fit. If there was any doubt about this before the decision in Barnwell, it has now been firmly dispelled. When an authority finds that a proposed development would harm the setting of a listed building or the character or appearance of a conservation area or a Historic Park, it must give that harm considerable importance and weight. This does not mean that an authority’s assessment of likely harm to the setting of a listed building or to a conservation area is other than a matter for its own planning judgment. It does not mean that the weight the authority should give to harm which it considers would be limited or less than substantial must be the same as the weight it might give to harm which would be substantial. But it is to recognise, as the Court of

Appeal emphasized in Barnwell, that a finding of harm to the setting of a listed building or to a conservation area gives rise to a strong presumption against planning permission being granted. The presumption is a statutory one, but it is not irrebuttable. It can be outweighed by material considerations powerful enough to do so. An authority can only properly strike the balance between harm to a heritage asset on the one hand and planning benefits on the other if it is conscious of the statutory presumption in favour of preservation and if it demonstrably applies that presumption to the proposal it is considering.

- 6.4.5 In short, there is a requirement that the impact of the proposal on the heritage assets be very carefully considered, that is to say that any harm or benefit needs to be assessed individually in order to assess and come to a conclusion on the overall heritage position. If the overall heritage assessment concludes that the proposal is harmful then that should be given "*considerable importance and weight*" in the final balancing exercise having regard to other material considerations which would need to carry greater weight in order to prevail.

*Impact on conservation area*

- 6.4.6 Paragraph 132 of the NPPF states that, '*When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation. The more important the asset, the greater the weight should be. Significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. As heritage assets are irreplaceable, any harm or loss should require clear and convincing justification.*'
- 6.4.7 Paragraph 134 of the NPPF goes on to say, '*where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use*'.
- 6.4.8 The Council, under saved UDP Policy CSV7 seeks to protect buildings within Conservation Areas, by refusing applications for their demolition or substantial demolition if it would have an adverse impact on the character and appearance of the Conservation Area. This should be considered alongside with London Plan Policies 3.5 and 7.6 and Local Plan Policy SP11, which identify that all development proposals should respect their surroundings by being sympathetic to their form, scale, materials and architectural detail.
- 6.4.9 London Plan Policy 7.8 requires that development affecting heritage assets and their settings to conserve their significance by being sympathetic to their form, scale and architectural detail. Haringey Local Plan Policy SP12 requires the conservation of the historic significance of Haringey's heritage assets. Saved Haringey Unitary Development Plan Policy CSV5 requires that alterations or extensions preserve or enhance the character of the Conservation Area.

- 6.4.10 The existing building at Nos.191 to 201 Archway Road is not statutorily listed or locally listed but the site does fall within Highgate Conservation Area and forms part of the sub-area 3 of the conservation area. The assessment of the application has had regard to the Highgate Conservation Area Appraisal and Management Plan adopted in December 2013.
- 6.4.11 The adopted Character Appraisal for the Highgate Conservation Area describes the site as follows, *“There are several small workshops which provide useful premises for small businesses, joinery and craft workshops. The shop front to Richardsons (antique dealers) is distinctive with a black granite shop frame and large glass windows broken only by black granite piers. There is a recessed clerestory with white opaque glass panel set in thin steel frames”*.
- 6.4.12 Archway Road forms sub-area 3 of the conservation area and is characterised by late 19<sup>th</sup> and early 20<sup>th</sup> Century terraced development of three storeys, mainly in red brick with decorative gables and rich architectural detailing. Within that, there is much variation along Archway Road itself such as the locally listed arched buildings on the southern end and more substantial and imposing four storey terraces towards the northern end near Jackson’s Lane Community Centre. The shops along Archway Road are much altered; however, many retain their original features underneath the later fascias and metal/plastic frames.
- 6.4.13 The application site at Nos. 191 to 201 Archway Road, also known as ‘Richardsons of Highgate’ due to the projecting shop on the ground floor, is an attractive terrace within the conservation area. Dating from the late 19<sup>th</sup> Century, these are built in a ‘stripped’ Victorian style with red bricks and canted bays to the front. The gables to the front contain terracotta finials between them and decorative ridge tiles. The front elevation is perhaps the most significant, making a positive contribution to the conservation area. In contrast, the rear and flank elevations are very simple in appearance with evidently different and possibly use of cheaper bricks. The workshops to the rear and the single storey extension to the north are utilitarian in form and therefore make a limited contribution to the conservation area.
- 6.4.14 Local residents and amenity groups have objected to the design, scale and impact on the conservation area.
- 6.4.15 The applicant held several pre-application meetings with Officers to discuss the acceptability of the design.
- 6.4.16 The scheme has been presented at Haringey Quality Review Panel. In summary, they broadly support the proposal including the restoration of the existing 19th century façade. They were also in the opinion that there was scope for further refinement in the architectural expression of new elements of the scheme, including the rebuilt shop front. The applicant has duly taken onboard these

comments and has revised the scheme to reflect the above. The amendments include the use of metal fins to the Causton Road frontage and courtyard elevations in order to match the profile of the roof line to retain the building form and enclose the amenity and entrance areas, and retaining and rebuilding the original shopfront features such as the stall riser, plinth, pilaster, concealed roller shutter and timber fascia board.

6.4.17 Whilst the scheme proposes to retain the outer shell of the existing building, it does propose a substantial extension to the rear that would have a greater impact on the conservation area than the existing smaller and ad hoc units it replaces. This extension proposal would be most visible from the Causton Road elevation, and would not be considered to preserve the character of this part of the conservation area. As per the Council's statutory duty, the limited harm has been given great weight in assessing whether the development preserves or enhances the conservation area.

6.4.18 The rear extension would be of a similar height as the existing front terrace and is designed to reflect the architectural treatment of the Archway Road façade, interpreted successfully in a contemporary manner. The pitched gables would be continued but in metal, evoking the tiled roof nostalgia of the existing terraces. Use of red brick would relate satisfactorily with the adjacent surroundings. Fenestration is high quality, maintaining existing proportions but modern in appearance. Recesses, dormers and chimneys add to the articulation of the façade and create an overall interesting skyline. As such, it is considered that the rear extension, although bigger in scale than the existing workshops, are no bigger than the existing scale of residential buildings and are of a high design quality that would positively enhance the conservation area.

6.4.19 In addition, considerable improvement to the front façade, including the replacement of the poor quality and rotten timber windows to the front with more appropriate and high quality windows is considered to be a heritage benefit. The retention of the facades and the 'retrofitting' of the building would allow for future sustainable use of the building and preserve the Archway Road frontage. The shop front which is in poor repair at present would also be improved and enhanced. As such this would be considered to provide considerable heritage benefits.

6.4.20 Overall, it is considered that the scheme provides a secure and sustainable use of the building providing additional housing, whilst preserving the most important facades and thus preserving its significance within the conservation area. Officers have taken a balanced view, having regard to Paragraphs 132 and 134 of the NPPF and concluded that the proposals result in less than substantial harm to the heritage assets caused by the scale of the extensions would be outweighed by the significant heritage benefits of the scheme. As such, the scheme would therefore be acceptable with regard to the Barnwell Manor case, the less than significant harm to the conservation area would therefore satisfy the



statutory duties set out in Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990, and accord to the design and conservation aims and objectives as set out in the NPPF, London Plan Policies 7.4, 7.5 and 7.6, saved UDP Policy UD3, Local Plan Policies SP11 and SP12 and SPG2 'Conservation and archaeology'.

## 6.5 Housing

### *Affordable housing*

- 6.5.1 The Council's Planning Policies as set out in Local Plan Policy SP2 requires that, "*Subject to viability, sites capable of delivering ten or more units, will be required to meet a borough wide affordable housing target of 50%, based on habitable rooms*". This stance is in line with London Plan Policy 3.8 which requires the provision of affordable family housing, where London Plan Policy 3.11 sets out the strategic affordable housing targets as it, "*seek to maximise affordable housing provision and ensure an average of at least 13,200 more affordable homes per year in London*".
- 6.5.2 London Plan Policy 3.12 states that Boroughs should seek, "*the maximum reasonable amount of affordable housing should be sought when negotiating on individual private residential and mixed use schemes*", *having regard to: their affordable housing targets; the need to promote mixed and balanced communities; the size and type of affordable housing needed in particular locations; and the individual circumstances including development viability*".
- 6.5.3 The policy further continues to say that, "*negotiations on sites should take account of their individual circumstances including development viability, the availability of public subsidy, the implications of phased development including provisions for reappraising the viability of schemes prior to implementation ('contingent obligations'), and other scheme requirements*".
- 6.5.4 Paragraph 173 of the NPPF seeks to ensure viability, so that, "*the costs of any requirements likely to be applied to development, such as requirements for affordable housing, standards, infrastructure contributions or other requirements should, when taking account of the normal cost of development and mitigation, provide competitive returns to a willing land owner and willing developer to enable the development to be deliverable*".
- 6.5.5 The application makes no affordable housing provision on-site. However, the applicant has submitted an economic viability assessment to justify this position. The applicant's viability appraisal considered two proposal options (B1 and D2) of the lower ground floor in order to establish the maximum level of planning obligations the scheme can provide whilst remaining commercially viable. The Council did not support the D2 option and as such this was discounted. Both of the two options produced a deficit when measured against the benchmark land

value. This suggests that the proposed development cannot reasonably support any affordable housing in addition to CIL contributions.

- 6.5.6 The report has been independently reviewed on behalf of the Council and this assessment concludes that the assumptions adopted by the applicant including the interest rate, contingency and construction costs are reasonable. The provision of affordable workspace has an impact on viability but is considered to be central to the acceptability of the scheme and the retention of the facade and facade works also impact on viability but these are considered to be reasonable. On this basis the independent assessment has concluded that the applicant could make an affordable housing payment in lieu of £50,000 when measured against the benchmark land value. Instead, the applicant is willing to accept a level of profit below 20% and has offered a commuted sum of £255,000. This is considered to be the maximum reasonable amount of contribution that the proposal can support

#### *Housing mix*

- 6.5.7 London Plan Policy 3.8 requires new residential developments to offer a range of housing choices, in terms of the mix of housing sizes and types, taking account of the housing requirements of different groups and the changing roles of different sectors, including the private rented sector.
- 6.5.8 The proposal is for 25 residential units. The housing mix is as follows:

<b>No. of bedrooms</b>	<b>No. of units</b>	<b>% of units</b>
1 bed units	6	24
2 bed units	12	48
3 bed units	7	28
<b>TOTAL</b>	<b>25</b>	<b>100</b>

- 6.5.9 Although the proposed housing mix has a larger number of 2 bedroom units (48%), this is offset by the quantum of family housing offered (28%). Furthermore, the Council has identified a shortage of family sized housing in the west of the borough and this development therefore addresses this by providing a number of 3 bed units on the site. Therefore the proposed mix of housing units is considered acceptable.

## **6.6 Impact on the amenity of adjoining occupiers**

- 6.6.1 Saved UDP Policy UD3 states that development proposals are required to demonstrate that there is no significant adverse impact on residential amenity or other surrounding uses in terms of loss of daylight or sunlight, privacy, overlooking. Similarly London Plan Policy 7.6 requires buildings and structures

should not cause unacceptable harm to the amenity of surrounding land and buildings, particularly residential buildings, in relation to privacy.

6.6.2 Local residents have objected to the proposal as they allege that it will lead to a reduction in existing levels of privacy, daylight and sunlight to adjacent residential properties.

6.6.3 The nearest existing residential properties that would be most affected by the siting and scale of the proposed development are:

- No. 187 to 189 Archway Road to the south;
- No. 2 Causton Road to the west; and
- No. 203 Archway Road to the north

*Daylight/sunlight*

6.6.4 In support of their application, the applicant has provided a daylight/sunlight report in line with Building Research Establishment (BRE) 2011 guidelines, British Standard BS 8206:2008 Lighting for buildings and Planning Practice Guidance (2014) - Design. Daylight is measured by Vertical Sky Component (VSC) whereas the acceptable level of sunlight is calculated by Annual Probable Sunlight Hours (APSH). The BRE Report suggests a VSC of 27% or more should be achieved if a room is to be adequately day lit. In terms of sunlight, the acceptability criteria are greater than 25% for the whole year or more than 5% between 21<sup>st</sup> September and 21<sup>st</sup> March. Only the existing habitable rooms of the neighbouring buildings are considered for the purposes of the BRE calculation.

Receptor	Floor	Window	Annual			Winter		
			Existing	Proposed	Ratio	Existing	Proposed	Ratio
203 Archway Road	G	1	58	46	0.79	18	16	0.89
203 Archway Road	G	2	55	40	0.73	20	13	0.65
203 Archway Road	G	3	22	11	0.50	15	6	0.40
203 Archway Road	G	4	22	9	0.41	14	2	0.14
203 Archway Road	1	1	75	69	0.92	71	61	0.86
203 Archway Road	1	2	71	61	0.86	24	20	0.83
203 Archway Road	1	3	63	54	0.86	25	18	0.72
203 Archway Road	2	1	81	81	1.00	26	26	1.00
203 Archway Road	3	1	83	83	1.00	28	28	1.00
203 Archway Road	3	2	83	83	1.00	28	28	1.00

Annual Probably Sunlight Hours (Existing and Proposed)

Receptor	Floor	Window	Vertical Sky Component (%)		Ratio
			Existing	Proposed	
187-189 Archway Road	1	1	36.02	35.05	0.97
2 Causton Road	G	1	28.47	28.09	0.99
2 Causton Road	G	2	21.22	17.20	0.81
2 Causton Road	1	1	38.29	38.11	1.00
2 Causton Road	1	2	38.58	38.49	1.00
2 Causton Road	1	3	38.25	38.09	1.00
2 Causton Road	1	4	38.44	38.33	1.00
2 Causton Road	2	1	37.59	37.59	1.00
203 Archway Road	G	1	24.42	17.59	0.72
203 Archway Road	G	2	20.00	12.10	0.60
203 Archway Road	G	3	9.18	2.49	0.27
203 Archway Road	G	4	10.98	4.51	0.41
203 Archway Road	1	1	33.33	27.64	0.83
203 Archway Road	1	2	31.13	24.04	0.77
203 Archway Road	1	3	28.00	20.67	0.74
203 Archway Road	2	1	36.56	34.50	0.94
203 Archway Road	3	1	38.67	38.23	0.99
203 Archway Road	3	2	38.74	38.37	0.99

Vertical Sky Component (Existing and Proposed)

- 6.6.5 The applicant's daylight/sunlight report concludes that the proposed development will not cause any adverse or significant impacts on any of the windows at Nos. 187 to 189 Archway Road or No. 2 Causton Road and the daylight levels will remain acceptable. It further notes that the windows on the ground floor at No. 203 Archway Road are already compromised by the staircase which leads to the upper floors of the building, and the windows on the upper floors at No. 203 Archway Road will not be significantly impacted based on the proposed calculations.
- 6.6.6 Officers have reviewed the report and it is noted that the existing second ground floor window of 2 Causton Road (21.22%) is below the standard 27% VSC requirements. When existing levels of daylight are below 27% VSC, a reduction of more than 20% from the existing level will be noticeable to the inhabitants, i.e. an impact will occur. In this case the proposed VSC value (17.2% represent a 19% reduction which is within the acceptable threshold. The proposal will not result an acceptable loss of daylight to 2 Causton Road in this regard.
- 6.6.7 The ground floor windows of 203 Archway Road currently experience deprived levels of daylight principally caused by the external bricked staircase attached to the side of the building. As such predicted VSC values are acceptable given the

current levels of daylight enjoyed by these ground floor windows and the fact that they are already likely to require electric lighting. The proposal will cause two out of the four first floor windows to fall below the 27% VSC minima. However, an inspection of the site reveals that these two affected central windows to the side elevations are likely to be non-habitable.

- 6.6.8 The proposal will maintain an acceptable level of daylight to the adjoining properties at Nos. 187 to 189 Archway Road as the proposed VSC value (35.05) will exceed the 27% requirement. The proposal therefore would not create any adverse daylight impact to 187 to 189 Archway Road.
- 6.6.9 The potentially affected windows of 2 Causton Road and 187 to 189 Archway Road do not face within 90 degrees of due south and therefore are not included as part of the sunlight assessment.
- 6.6.10 In terms of potential sunlight impact upon 203 Archway Road, the proposal will cause two ground floor windows to fail the APSH criteria. However as noted in the daylight assessment, these affected windows are already adversely impacted by the staircase above them.
- 6.6.11 As a summary, and taking into account all the adjacent residential units namely: 187 to 189 Archway Road; 2 Causton Road to the west; and 203 Archway Road, the proposed development would satisfy the daylight and sunlight BRE recommendations in maintaining an acceptable level of living conditions currently enjoyed by habitants of the those properties in meeting saved UDP Policy UD3 and London Plan Policy 7.6 which amongst other aims seek to safeguard existing amenity conditions.

#### *Privacy*

- 6.6.12 Local residents living in adjacent properties have raised concerns of overlooking and loss of privacy in objecting to the proposal.
- 6.6.13 The siting and orientation of the habitable room windows proposed have been carefully sited so as to maintain acceptable levels of privacy currently enjoyed by occupiers living at 2 Causton Road (west) and 203 Archway Road (north). The upper floor bedroom windows to the northern elevation will face the staircase and non-habitable window at 203 Archway Road. Similarly the west-facing bedroom windows will face the flank wall of 2 Causton Road.
- 6.6.14 The development would not have any material adverse impacts on surrounding residents and occupiers within regards to enclosure, loss of outlook or excessive noise levels.

## **6.7 Living conditions for future occupants**

- 6.7.1 Local Plan Policy SP2, London Plan Policy 3.5 and the Mayor's Housing Supplementary Planning Guidance (SPG), November 2012, set out the minimum unit sizes for new residential development proposals to ensure an acceptable level of living accommodation offered.
- 6.7.2 In assessing the proposal against the above requirements, all the 1 bedroom, 2 bedroom and 3 bedroom units would accord with the minimum unit size requirements (50sqm to 86sqm) as laid out in the London Plan.
- 6.7.3 The London Plan further gives guidance on the minimum individual room sizes and amenity space for the residential development proposals. In line with the London Plan space standards, all the individual rooms and the private amenity space afforded to the individual flats meet the minimum threshold to result in an acceptable level of residential accommodation for future occupants of the new development in accordance to Local Plan Policy SP2, London Plan Policy 3.5 and the Mayor's Housing Supplementary Planning Guidance.
- 6.7.4 In addition to meeting the space standards, all the individual units are dual aspect and there will be no direct overlooking between the units around the communal deck access as the principal elevations of the adjacent blocks are orientated perpendicular to one another. There is a change of floor finish in front of the bedrooms facing the external deck access to provide defensible space in front of them.
- 6.7.5 Overall, the proposal will provide acceptable living conditions for future occupiers of the new development in accordance to Local Plan Policy SP2, London Plan Policy 3.5 and the Mayor's Housing Supplementary Planning Guidance.

## **6.8 Parking and highway safety**

- 6.8.1 Local Plan Policy SP7 recognises the need to minimise congestion and addressing the environmental impacts of travel. London Plan Policy 6.3 requires development proposal to the impacts on transport capacity and the network should be taken into account.
- 6.8.2 The application site has a public transport accessibility level (PTAL) rating of 4 indicative of good accessibility to local public transport services including a number of local bus routes along Archway Road and Highgate Underground Station. Part of the site fronting onto Archway Road forms part of the TLRN (Transport for London Route Network) A1 route and is subject to 'red route' parking restrictions.
- 6.8.3 Similarly, the section of Causton Road that adjoins the development site is subject to 'red route' parking controls Monday to Friday 07:00 to 19:00 on the

adjoining kerbside. The parking restrictions on the opposite kerbside in Causton Road consists of red lines (with restrictions as above) and two parking bays with a capacity for three cars, that allows parking for 1 hour maximum and no return within two hours. Further along Causton Road the on-street parking bays are included in a controlled parking zone (CPZ) which operates Monday to Friday 10:00 to 12:00. The CPZ parking spaces in Causton Road are also resident permit holders only.

- 6.8.4 The current parking access arrangement is such that vehicular access to the informal courtyard car park that can accommodate up to 8 cars to the rear of the site is obtained from Archway Road. Elsewhere, Causton Road provides access for deliveries, servicing and refuse collection for the existing uses. There are two existing vehicle crossovers on the Causton Road frontage of the site, which are utilised for bringing in/out goods/refuse from the premises. Pedestrian access is taken from Archway Road and Causton Road.
- 6.8.5 The proposal includes provision for 7 courtyard car parking spaces, which includes 3 disabled car parking spaces and 2 car club bays. The proposed allocation of car parking is 5 car parking spaces (including the 3 disabled car parking spaces) for the residential element of the development and the 2 car club bays to be available for use by occupants of the development and the public, complementing the car club bay provision in the locality. It should be noted that the proposed car parking spaces is broadly the same as the existing development. Access to the car park will be taken via the existing vehicle crossover in Archway Road. The level of car parking is acceptable and is consistent with London Plan Policy 6.13 and Local Plan Policy SP7.
- 6.8.6 Servicing and deliveries will continue to be undertaken in Causton Road as existing. The transport statement does not include any data on the number of servicing and delivery trips under the existing development. The delivery trip prediction under the proposal is 30 deliveries per week, which equates to an average of 4 vehicles per day. Of these 30 deliveries 9 deliveries per week will be by 10m or 13.4m articulated vehicles; 7 deliveries by 6m rigid vehicles; 7 deliveries by large vans; and 7 deliveries by small vans.
- 6.8.7 However, Officers do not consider Causton Road is suitable for deliveries by articulated lorries as they would either have to access Causton Road in a forward direction and reverse onto Archway Road on leaving the site or vice versa. This manoeuvre would be detrimental to the adjoining road network and therefore it is recommended that delivery be limited to rigid vehicles that can access Causton Road without reversing from or onto Archway Road. The Council therefore recommends the implementation of a delivery and servicing management plan (DSP) on occupation of the development, in the interest of minimising impacts on traffic in the adjoining road network.

- 6.8.8 The transport assessment includes a trip generation analysis which compares the existing and proposed development in order to determine the residual trip generation of the proposal. An additional 38 and 21 two-way vehicle trips in the AM and PM peak traffic periods, respectively, is predicted under the proposal. The increase in vehicle trips will not be detrimental to the operation of the adjoining road network. Pedestrian trips will account for the largest increases in trips under the proposal – 318 and 400 trips during the AM and PM peaks respectively. The additional pedestrian trips can be accommodated within the adjoining pedestrian infrastructure. Public transport will account for additional 97 and 221 two-way trips during the AM and PM peak traffic periods, respectively. The increase in public transport trips can be accommodated within the capacity of the existing public transport provision.
- 6.8.9 The transport assessment includes the results of parking stress surveys which were undertaken at night in May 2015. The survey covered on-street car parking within 200m radius of the site, consistent with the Lambeth Parking Survey Methodology. The applicant was asked to undertake a further parking survey during the day when the commercial uses in the area are active and when the gym would be at its busiest. A parking survey was undertaken in the afternoon in September 2015. The latest parking survey observed that 31 out of 39 parking spaces which allow parking for duration of 1 hour during the restricted hours (07:00 to 19:00) were available.
- 6.8.10 The results of the survey also indicated varying levels of parking stress in the streets surveys. The largest spare capacity observed is Archway Road, which has a capacity of 39 car parking spaces; ignoring the 3 bays which allow loading for 20 minutes between 10:00 to 16:00 and 7 bays with no signs indicating restricted times.
- 6.8.11 Officers consider the proposed B1 use would not give rise to any significant increase in parking stress. The operation of parking restrictions in the adjoining streets between 10:00 and 12:00 will discourage staff from commuting to work by car. The lack of available on-street parking where staff can park throughout the day should ensure that minimal parking effects will be created by the proposed B1 use. As such, the B1 proposal will not prejudice the local road network generally.
- 6.8.12 The development provides a total of 58 cycle parking spaces. 44 cycle parking spaces will be provided the residential use and 14 spaces for the commercial uses. The quantum of residential cycle parking is in line with the London Plan cycle parking standards, and is located on the ground floor adjacent to the lift and external stairs. Cycle parking for the commercial use is proposed in the form of Sheffield Stands. 7 Sheffield Stands are proposed on the adjoining footway in Causton Road. However, this falls short of the London Plan standards which requires a total of 15 short-stay cycle parking spaces Given this shortfall, the



Council will therefore seek to increase the proposed short-stay cycle parking and a financial contribution of £3,291 (£318/cycle stand x 9 x 15%) towards the cost of providing the proposed short-stay cycle parking on the adjoining public realm will be sought and secured via a legal agreement.

- 6.8.13 Adequate secure and sheltered cycle parking for the commercial uses is required. Details of the long-stay cycle parking for the commercial use must be provided for the approval of the Council prior to occupation of the commercial uses. This will be secured by condition.
- 6.8.14 The proposal will necessitate improvements to the adjoining highway, such as footway resurfacing, removal of the existing crossovers in Causton Road, and the installation of the proposed cycle parking stands on the corner of Archway Road/ Causton Road. The applicant will be required to enter into S278 agreement to pay the Council for the above highway improvement works, and the imposition of a condition to the decision would ensure compliance.
- 6.8.15 In light of the above evaluation and subject to the signing of a S106 agreement to secure a 'car free' development, local car club membership and commercial cycle parking, and for conditions requesting servicing details of the future commercial unit - the proposal would not have a detrimental impact on general amenity and surrounding highway network in accordance to Local Plan Policy SP7 and London Plan Policy 6.3.

## **6.9 Accessibility**

- 6.9.1 The NPPF and London Plan policies 3.8 and 7.2 and Local Plan policy SP2 require all development proposals to provide satisfactory access for disabled people and those with mobility difficulties such as parents with pushchairs and young children. All residential units should be built in accordance with Lifetime Homes Standards (LTH) and Part M of Building Regulations to ensure any new housing development is suitable for the disabled users.
- 6.9.2 The applicant has recognised the need to meet Lifetime Homes and Approved Document M of the Building Regulations in their design and access statement submission. The individual and communal door entrances are wide enough and level (Criterion 3 and 4), to facilitate ease of entry for disabled users and those with mobility difficulties'. A 300mm leading edge has been achieved to all doors and all doors/hallways will achieve the minimum effective clear widths within the individual units (Criterion 4 and 6). A level entry WC which has the potential for showering facilities has been provided for the individual flats (Criterion 10). The bedroom and bathroom of the units have the potential for future fitting of hoists (Criterion 13). The bathrooms have been designed for ease of access (Criterion 14). The full height living room windows also mean occupiers are able to have a reasonable outlook when seated. (Criterion 15).

6.9.3 The proposal makes provision for 3 units, each located on the first, second and third floors, accessed via a lift in the central core of the building that are capable of being adapted in line with wheelchair accessible requirements. Each unit has been designed to the GLA Wheelchair Accessible Housing 'Best Practice Guidance' document. The total number of 3 accessible units provided (13%) exceeds the 10% Local Plan and London Plan requirement in order to meet the needs of future wheelchair occupants. The wheelchair accessible units have been designed to include a dedicated charging point/parking at the entrance and an accessible bathroom to facilitate a 1500mm turning circle which is also adjacent to a bedroom for a future potential door. The wheelchair accessible units will also be allocated each a single disabled parking bay as required by the London Plan.

## 6.10 Trees

6.10.1 The site lies within a conservation area and as such all trees within the conservation area are protected. The supporting text to Local Plan Policy SP13 recognises, "*trees play a significant role in improving environmental conditions and people's quality of life*", where the policy in general seeks the protection, management and maintenance of existing trees.

6.10.2 Part e) of saved UDP Policy UD3 states that the Council will require development proposals to consider appropriate tree retention, where UDP Policy OS17 seeks to protect and improve the contribution of trees, tree masses and spines to local landscape character.

6.10.3 There are currently no trees on the application site. However 13 offsite trees located in the front and rear gardens of the properties No. 203 Archway Road and No. 2 Causton Road were surveyed. Of the trees surveyed: 1 is category A (High Quality); 7 are B category (Moderate Quality); and 5 are C category (Low Quality). The retention of the existing wall, including the basement walls ensures any impacts on the off-site trees are kept minimal. Only the offsite category B ash trees, T6 and T7, both located in the front garden of 203 Archway Road are considered to be the most affected by the proposal through the demolition of existing building/removal of existing hard surfaces and replacement surfaces within the root protection areas of these identified trees. Mitigation measures are proposed as set out within the arboricultural report, including manual demolition/removal of the existing building/hard surfaces, the retention of the existing sub-base to allow no-dig construction of the replacement surface, and the use of low invasive foundations for any proposed boundary fencing, to ensure the impact to these trees is low. These measures are considered acceptable by Officers in order to maintain the well being of the offsite trees and the visual amenity of the general area in meeting Local Plan Policy SP13, saved UDP Policy UD3 and UDP Policy OS17.

## 6.11 Sustainability

- 6.11.1 The NPPF, London Plan and local policies require development to meet the highest standards of sustainable design, including the conservation of energy and water; ensuring designs make the most of natural systems and the conserving and enhancing the natural environment. Chapter 5 of the London Plan requires all new homes to meet Level 4 of the Code for Sustainable Homes. Notwithstanding the above policy context, recent Government announcements have meant that Local Planning Authorities can no longer require developers to achieve the minimum Code requirements as this has now been absorbed within Building Regulations. On the other hand, there is still a requirement for the scheme to achieve a BREEAM 'Very Good' standard under the BREEAM New Construction (2014). This will be secured by condition.
- 6.11.2 London Plan Policy 5.2 requires major residential proposals to attain a 40 per cent carbon dioxide emissions improvement on 2010 Building Regulations Part L, and such major developments should include an energy assessment to demonstrate how the carbon dioxide emissions reduction targets are met.
- 6.11.3 The energy baseline for the development proposal would have emitted 187.3 tonnes of CO<sub>2</sub> per year if building regulations compliant. The scheme is required to deliver a carbon saving of 40% or a new target emission of 149.8 tonnes of CO<sub>2</sub> per year. The development delivers a new emissions figure of 158.1 tonnes of CO<sub>2</sub> per year which represents a shortfall of 8.3 tonnes. As such the development will be expected to offset the remaining 8.3 tonnes of carbon. Based on the assumption cost of £2,700 per tonne of carbon over 30 years - a contribution of £22,410 to the Councils carbon offsetting fund will be sought and secured under a S106 Legal Agreement.
- 6.11.4 Officers welcome that a single heating and hot water network served from a single energy centre across all elements of the development (office and residential) is proposed. However further details are required on how this single energy centre will be able to connect to a community heating network at a later date as well as maps of the energy centre location, pipe routes and technical specification. These details will be sought by condition.

## **6.12 Flood Risk**

- 6.12.1 Local Plan Policy SP5 and London Plan Policy 5.12 seek to address current and future flood issues and minimise risks in a sustainable and cost effective way.
- 6.12.2 London Plan Policy 5.13 sets out the drainage hierarchy for Sustainable Drainage Systems (SUDS) so greenfield run-off rates are achieved and that surface water run-off is managed as close to its source as possible:
1. store rainwater for later use;
  2. use infiltration techniques, such as porous surfaces in non-clay areas;

3. attenuate rainwater in ponds or open water features for gradual release;
- 4 attenuate rainwater by storing in tanks or sealed water features for gradual release;
- 5 discharge rainwater direct to a watercourse;
- 6 discharge rainwater to a surface water sewer/drain; and
- 7 discharge rainwater to the combined sewer

6.12.3 The site predominantly falls within flood risk zone 1 which indicates low probability of flooding which comprises land assessed as having a less than 1 in 1,000 annual probability of river or sea flooding (<0.1%).

6.12.4 Officers consider that the development by reason of being located within flood risk zone 1, the existing buildings and hardstanding and the comprehensive landscaping scheme proposed will not increase flood risk on or off the site in accordance with Local Plan Policy SP5 and London Plan Policy 5.12.

6.12.5 Thames Water has set out that it has been unable to determine the waste water infrastructure needs of this application given the information submitted. It requested that the Local Planning Authority include a 'Grampian Style' condition - *“Development shall not commence until a drainage strategy detailing any on and/or off site drainage works, has been submitted to and approved by, the local planning authority in consultation with the sewerage undertaker. No discharge of foul or surface water from the site shall be accepted into the public system until the drainage works referred to in the strategy have been completed”*. This condition requested by Thames Water has been included on the draft decision notice.

### **6.13 Section 106**

6.13.1 This application will be subject to a S106 legal agreement and the applicant has agreed to the following heads of terms:

- i. £255,000 towards affordable housing.
- ii. £1,000 towards the amendment of the TMO to secure the 'car free' development, and two years free membership to a local Car Club and £50 free credit per unit.
- iii. £3,291 towards short-stay cycle parking on the adjoining public realm.
- iv. £22,410 to the Council's carbon offsetting fund.
- v. Affordable B1 workspace – capping rents.
- vi. Participation in the Council's employment initiatives during construction phase.
- vii. Considerate constructors' scheme.

### **6.14 Conclusion**

- 6.14.1 The proposed development is considered acceptable in principle in this instance as it would provide residential dwellings and additional family-sized housing generally whilst contributing to the Borough's housing targets as set out in Haringey's Local Plan and the London Plan.
- 6.14.2 The loss of the existing low quality workshop units is acceptable as they will be replaced by higher quality employment generating provision in the form of flexible and affordable B1 workspace.
- 6.14.3 The proposed density of 78 units per hectare and 238 habitable rooms per hectare is of an acceptable density for the site as it falls within the appropriate density range as set out in the London Plan for this part of the Borough.
- 6.14.4 The proposed development would not cause any material loss of amenity of that currently enjoyed by existing and surrounding occupiers and residents of Causton Road and Archway Road in terms of outlook, enclosure, and loss of daylight/sunlight, overshadowing, loss of privacy or overlooking.
- 6.14.5 The proposals involve extensions to the rear and side of Causton Road. Although the proposals will cause some visual harm to the character the conservation area the harm is considered to be less than substantial. This harm has been given considerable weight by officers but it is outweighed by the significant heritage benefits of the scheme as a whole.
- 6.14.6 The development makes provision for wheelchair accessible units and has been designed to meet Lifetime Homes standards, and provides an acceptable level of living accommodation and amenity space for future occupiers of the new development.
- 6.14.7 A number of conditions have been suggested should any consent be granted requesting details of the construction management plan and servicing of the new commercial unit to ensure it does not prejudice existing road and parking conditions, namely vehicular movements along Archway Road, Causton Road and the local road network generally and would not have an adverse impact on pedestrian safety.
- 6.14.8 All other relevant policies and considerations, including equalities, have been taken into account. Planning permission should be granted for the reasons set out above. The details of the decision are set out in the RECOMMENDATION

## **7.0 CIL**

- Based on the information given on the plans, the Mayor's CIL charge will be £25,585 (731 x £35) and the Haringey CIL charge will be £193,715 (731 x £265). This will be collected by Haringey after the scheme is implemented and could be

subject to surcharges for failure to assume liability, for failure to submit a commencement notice and/or for late payment, and subject to indexation in line with the construction costs index. An informative will be attached advising the applicant of this charge.

## 8.0 RECOMMENDATIONS

GRANT PERMISSION subject to conditions and subject to sec. 106 Legal Agreement

Subject to the following condition(s)

1. The development hereby authorised must be begun not later than the expiration of 3 years from the date of this permission, failing which the permission shall be of no effect.

Reason: This condition is imposed by virtue of Section 91 of the Town & Country Planning Act 1990 and to prevent the accumulation of unimplemented planning permissions.

2. The development hereby authorised shall be carried out in accordance with the following approved plans and specifications:

- 499-0000-GA Rev 1 (Existing Site Location Plan)
- 499-0001-GA Rev 1 (Existing Site Plan)
- 499-0010-GA Rev 1 (Existing Ground Floor Plan)
- 499-0011-GA Rev 1 (Existing First Floor Plan)
- 499-0012-GA Rev 1 (Existing Second Floor Plan)
- 499-0013-GA Rev 1 (Existing Third Floor Plan)
- 499-0020-GA Rev 1 (Existing Basement Floor Plan)
- 499-0030-GA Rev 1 (Existing Section AA)
- 499-0031-GA Rev 1 (Existing Section BB)
- 499-0040-GA Rev 1 (Existing North East Elevation)
- 499-0041-GA Rev 1 (Existing North West Elevation)
- 499-0042-GA Rev 1 (Existing South East Elevation)
- 499-0043-GA Rev 1 (Existing South West Elevation)
- 499-0100-GA Rev 1 (Proposed Site Location Plan)
- 499-0110-GA Rev 1 (Demolition Ground Floor Plan)
- 499-0111-GA Rev 1 (Demolition First Floor Plan)
- 499-0112-GA Rev 1 (Demolition Second Floor Plan)
- 499-0113-GA Rev 1 (Demolition Third Floor Plan)
- 499-0120-GA Rev 1 (Demolition Basement Floor Plan)
- 499-0130-GA Rev 1 (Demolition Section AA)
- 499-0131-GA Rev 1 (Demolition Section BB)
- 499-0140-GA Rev 1 (Demolition North East Elevation)
- 499-0141-GA Rev 1 (Demolition North West Elevation)
- 499-0142-GA Rev 1 (Demolition South East Elevation)

- 499-0143-GA Rev 1 (Demolition South West Elevation)
- 499-0200-GA Rev 13 (Proposed Ground Floor Plan)
- 499-0201-GA Rev 7 (Proposed First Floor Plan)
- 499-0202-GA Rev 7 (Proposed Second Floor Plan)
- 499-0203-GA Rev 7 (Proposed Third Floor Plan)
- 499-0204-GA Rev 2 (Proposed Roof Plan)
- 499-0210-GA Rev 12 (Proposed Basement Plan)
- 499-0220-GA (Proposed Cycling Provision)
- 499-0300-GA Rev 2 (Proposed Section AA)
- 499-0301-GA Rev 2 (Proposed Section BB)
- 499-0302-GA Rev 2 (Proposed Section CC)
- 499-0303-GA Rev 2 (Proposed Section DD)
- 499-0304-GA Rev 2 (Proposed Section EE)
- 499-0400-GA Rev 3 (Proposed North East Elevation)
- 499-0401-GA Rev 3 (Proposed North West Elevation)
- 499-0402-GA Rev 3 (Proposed South West Elevation)
- 499-0403-GA Rev 3 (Proposed South East Elevation)
- Arboricultural Impact Assessment Report ref. PSP/191ACR/AIA/01a
- Daylight and Sunlight Assessment ref. A2500/DS/001
- Design and Access Statement dated August 2015
- Energy Statement & BREEAM Pre-assessment dated 4th June 2015
- Framework Travel Plan ref. MTP Ref: 15/025
- Heritage Statement dated August 2015
- Noise Assessment ref. A2500/N/002
- Planning Statement dated August 2015
- Transport Statement ref. MTP Ref: 15/025

Reason: In order to avoid doubt and in the interests of good planning.

3. Notwithstanding the description of the materials in the application, no development (with the exception of demolition) shall take place until precise details of the materials to be used in connection with the development hereby permitted be submitted to, approved in writing by and implemented in accordance with the requirements of the Local Planning Authority and retained as such in perpetuity.

Reason: In order to retain control over the external appearance of the development in the interest of the visual amenity of the conservation area.

4. No development of the shopfront hereby approved shall commence until details of the new shop front, signage and illumination have been submitted to, and approved in writing by, the Local Planning Authority.

Reason: In order to retain control over the external appearance of the development in the interest of the visual amenity of the conservation area.

5. No development (with the exception of demolition) hereby approved shall commence until full details of both hard and soft landscape works, have been submitted to, and approved in writing by the Local Planning Authority and these works shall be carried out as approved. These details shall include: proposed finished levels or contours; means of boundary fencing / railings; car parking layouts; other vehicle and pedestrian access and circulation areas; hard surfacing materials; minor artefacts and structures (e.g. furniture, play equipment, refuse or other storage units, signs, lighting etc.); proposed and existing functional services above and below ground (e.g. drainage power, communications cables, pipelines etc. indicating lines, manholes, supports etc.); retained historic landscape features and proposals for restoration, where relevant.

Soft landscape works shall include planting plans; written specifications (including cultivation and other operations associated with plant and grass establishment); schedules of plants, noting species, plant sizes and proposed numbers/densities where appropriate; implementation programme).

Such an approved scheme of planting, seeding or turfing comprised in the approved details of landscaping shall be carried out and implemented in strict accordance with the approved details in the first planting and seeding season following the occupation of the building or the completion of development (whichever is sooner). Any trees or plants, either existing or proposed, which, within a period of five years from the completion of the development die, are removed, become damaged or diseased shall be replaced in the next planting season with a similar size and species. The landscaping scheme, once implemented, is to be retained thereafter.

Reason: In order for the Local Planning Authority to assess the acceptability of any landscaping scheme in relation to the site itself, thereby ensuring a satisfactory setting for the proposed development in the interests of the visual amenity of the area

6. The A1 use forming part of the development hereby permitted shall not be operated before 07:00 hours or after 23:00 hours Monday to Sundays and Bank Holidays.

Reason: This permission is given to facilitate the beneficial use of the premises whilst ensuring that the amenities of adjacent residential properties are not diminished.

7. The B1 use forming part of the development hereby permitted shall not be operated before 07:00 hours or after 21:00 hours Monday to Sundays and Bank Holidays.



Reason: This permission is given to facilitate the beneficial use of the premises whilst ensuring that the amenities of adjacent residential properties are not diminished.

8. Prior to installation, details of the Ultra Low NOx boilers for space heating and domestic hot water must be submitted to, and approved in writing by the Local Planning Authority. The boilers to be provided for space heating and domestic hot water shall have dry NOx emissions not exceeding 20 mg/kWh.

Reason: To ensure that the development achieves a high level of sustainability

9. No development hereby approved shall commence until details of the community heat boilers have been submitted to, and approved in writing by the Local Planning Authority. Evidence shall demonstrate the unit to be installed complies with the emissions standards as set out in the GLA SPG Sustainable Design and Construction for Band A.

Reason: To ensure that the development achieves a high level of sustainability

10. No development hereby approved shall commence until details of a detailed Air Quality and Dust Management Plan (AQDMP), detailing the management of demolition and construction dust, have been submitted to, and approved in writing by the Local Planning Authority. The plan shall be in accordance with the GLA SPG Dust and Emissions Control and shall also include a Dust Risk Assessment.

Reason: To ensure that the development achieves a high level of sustainability

11. No development (with the exception of demolition) hereby approved shall commence until a Contractor Company is registered with the Considerate Constructors' Scheme. Proof of registration must be submitted to the Local Planning Authority.

Reason: To safeguard local amenity.

12. No development hereby approved shall commence until all plant and machinery to be used at the demolition and construction phases have been submitted to, and approved in writing by, the Local Planning Authority. Evidence is required to meet Stage IIIA of EU Directive 97/68/ EC for both NOx and PM. No works shall be carried out on site until all Non-Road Mobile Machinery (NRMM) and plant to be used on the site of net power between 37kW and 560 kW has been registered at <http://nrmm.london/>. Proof of registration must be submitted to the Local Planning Authority prior to the commencement of any works on site.

Reason: To protect local air quality.

13. An inventory of all NRMM must be kept on site during the course of the demolitions, site preparation and construction phases. All machinery should be regularly serviced and service logs kept on site for inspection. Records should be kept on site which details proof of emission limits for all equipment. This documentation should be made available to local authority officers as required until development completion.

Reason: To protect local air quality.

14. No development hereby approved in relation to the below elements shall commence until operational details of the heat network (pressures and temperatures) have been submitted to, and approved in writing by, the Local Planning Authority. The location of the energy centre shall ensure that there is space for future heat exchangers should the network not be delivered at this time. An identified route from the energy centre to the public highway shall be reserved for connectivity to the area wide network at a later date.

Reason: To ensure that the development achieves a high level of sustainability

15. No building shall be occupied until a final Certificate has been issued certifying that BREEAM (or any such equivalent national measure of sustainable building which replaces that scheme) rating 'Very Good' has been achieved for this development. Proof of final Certificate must be submitted to the Local Planning Authority.

Reason: To ensure that the development achieves a high level of sustainability

16. No development hereby approved in relation to the below elements shall commence until a drainage strategy detailing any on and/or off site drainage works, has been submitted to, and approved in writing by, the Local Planning Authority in consultation with the sewerage undertaker. No discharge of foul or surface water from the site shall be accepted into the public system until the drainage works referred to in the strategy have been completed.

Reason: The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community. Should the Local Planning Authority consider the above recommendation is inappropriate or are unable to include it in the decision notice, it is important that the Local Planning Authority liaises with Thames Water Development Control Department (telephone 0203 577 9998) prior to the Planning Application approval.

17. No development (with the exception of demolition) hereby permitted shall commence until detailed design and method statements (in consultation with London Underground) for all of the foundations, basement and ground floor structures, or for any other structures below ground level, including piling

(temporary and permanent), have been submitted to and approved in writing by the Local Planning Authority which:

- provide details on all structures
- accommodate the location of the existing London Underground structures and tunnels
- Accommodate ground movement arising from the construction thereof and mitigate the effects of noise and vibration arising from the adjoining operations within the structures and tunnels.

The development shall thereafter be carried out in all respects in accordance with the approved design and method statements, and all structures and works comprised within the development hereby permitted which are required by the approved design statements in order to procure the matters mentioned in paragraphs of this condition shall be completed, in their entirety, before any part of the building hereby permitted is occupied.

Reason: To ensure that the development does not impact on existing London Underground transport infrastructure,

18. a) No development hereby approved other than demolition to existing ground level shall take place until the applicant (or their heirs and successors in title) has secured the implementation of a programme of archaeological evaluation in accordance with a written scheme which has been submitted by the applicant and approved in writing by the Local Planning Authority and a report on that evaluation has been submitted to the Local Planning Authority.

b) If heritage assets of archaeological interest are identified by the evaluation under Part A, then before development, other than demolition to existing ground level, commences the applicant (or their heirs and successors in title) shall secured the implementation of a programme of archaeological investigation in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the local planning authority in writing.

c) No development or demolition shall take place other that in accordance with the Written Scheme of Investigation approved under Part (b).

d) The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under Part (b), and the provision for analysis, publication and dissemination of the results and archive deposition has been secured.

Reason: Heritage assets of archaeological interest may survive on the site. The planning authority wishes to secure the provision of appropriate archaeological

investigation, including the publication of results, in accordance with Section 12 of the NPPF

19. No development hereby approved shall commence until a Construction Management Plan (CMP) and Construction Logistics Plan (CLP) have been submitted to, and approved in writing by, the Local Planning Authority. The Plans should provide details on how construction work (including any demolition) would be undertaken in a manner that disruption to traffic and pedestrians on Archway Road and the surrounding residential roads is minimised. It is also requested that construction vehicle movements should be carefully planned and co-ordinated to avoid the AM and PM peak periods.

Reason: To reduce congestion and mitigate any obstruction to the flow of traffic on the transportation and Highways network.

20. Prior to the first occupation of the development, a Delivery and Servicing Plan (DSP) shall be submitted to, and approved in writing by, the Local Planning Authority. The DSP must be in place prior to operation of the development and to be modified in line with negotiated targets from time to time.

Reason: To reduce congestion and mitigate any obstruction to the flow of traffic on the transportation and Highways network.

21. The owner shall be required to enter into agreement with the Highway Authority (LB Haringey Council with respect to Causton Road and Transport for London with respect to Archway Road) under Section 278 of the Highways Act to pay for any necessary highway works, which includes if required, but not limited to, footway improvement works, access to the Highway, measures for street furniture relocation, carriageway markings, access and visibility safety requirements. Unavoidable works required to be undertaken by Statutory Services will not be included in LBH Haringey Estimate or Payment.

Reason: In the interest of highway safety and to protect the visual amenity of the locality.

22. Prior to the first occupation of the development the internal lockable space shall be made available within the building for the secure residential parking of 44 bicycles, as shown on the approved plans.

Reason: To ensure that a reasonable provision is made within the site for the parking of bicycles in the interest of relieving congestion in surrounding streets and improving highway conditions in general.

23. No development hereby approved shall be occupied until commercial cycle parking details has been submitted to, and approved in writing by the Local Planning Authority. Details of the parking shall be consistent with the recommendations of

the London Plan, and to be made available for staff of the commercial uses. The commercial units hereby approved shall not be occupied until the cycle parking has been implemented and shall be retained, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that a reasonable provision is made within the site for the parking of bicycles in the interest of relieving congestion in surrounding streets and improving highway conditions in general.

24. Prior to the first occupation of the residential units hereby approved, the car parking accommodation as shown on the approved plans shall be provided, and shall be retained in perpetuity for the accommodation of vehicles associated with the occupation of these residential units.

Reason: In the interests of orderly and satisfactory parking provisions being made on the site to ensure that the proposed development does not prejudice the free flow of traffic or public safety along the adjoining highway

### **Informatives:**

INFORMATIVE: In dealing with this application, Haringey Council has implemented the requirements of the National Planning Policy Framework and of the Town and Country Planning (Development Management Procedure) (England) (Amendment No.2) Order 2012 to work with the applicant in a positive and proactive manner. As with all applicants, we have made available detailed advice in the form of our statutory policies, and all other Council guidance, as well as offering a full pre-application advice service, so as to ensure the applicant has been given every opportunity to submit an application which is likely to be considered favourably.

INFORMATIVE: The applicant is advised that the proposed development will be liable for the Mayor of London's CIL. Based on the Mayor's CIL charging schedule and the information given on the plans, the Mayor's CIL charge will be £25,585 (731 x £35) and the Haringey CIL charge will be £193,715 (731 x £265). This will be collected by Haringey after the scheme is implemented and could be subject to surcharges for failure to assume liability, for failure to submit a commencement notice and/or for late payment, and subject to indexation in line with the construction costs index.

INFORMATIVE: The applicant is advised that under the Control of Pollution Act 1974, construction work which will be audible at the site boundary will be restricted to the following hours:-

8.00am - 6.00pm Monday to Friday

8.00am - 1.00pm Saturday and not at all on Sundays and Bank Holidays.

INFORMATIVE: The applicant's attention is drawn to the Party Wall Act 1996 which sets out requirements for notice to be given to relevant adjoining owners of intended works on a shared wall, on a boundary or if excavations are to be carried out near a neighbouring building.

INFORMATIVE: New shop front and signage should reflect the architectural detailing and character of the building and this should be applicable for future occupiers as well as owners of the units.

Signage should be customised including the adaptation of the corporate branding and lettering to be sensitive to the building and its context.

INFORMATIVE: Planning permission has been granted without prejudice to the need to get advertisement consent under the Town & Country Planning (Control of Advertisements) (England) Regulations 2007.

INFORMATIVE: The new development will require numbering. The applicant should contact the Local Land Charges team at least six weeks before the development is occupied (tel. 020 8489 5573) to arrange for the allocation of a suitable address

INFORMATIVE: Thames Water requests that the Applicant should incorporate within their proposal, protection to the property by installing for example, a non-return valve or other suitable device to avoid the risk of backflow at a later date, on the assumption that the sewerage network may surcharge to ground level during storm conditions.

Thames Water would recommend that petrol / oil interceptors be fitted in all car parking/washing/repair facilities. Failure to enforce the effective use of petrol / oil interceptors could result in oil-polluted discharges entering local watercourses.

Thames Water would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991.

A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Thames Water would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 02035779483 or by emailing

[wwriskmanagement@thameswater.co.uk](mailto:wwriskmanagement@thameswater.co.uk). Application forms should be completed on line via [www.thameswater.co.uk/wastewaterquality](http://www.thameswater.co.uk/wastewaterquality).

Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

Thames Water require a site drainage strategy that specifies current and proposed foul and surface water peak discharge rates and points of connection into the public sewer system. Thames Water expect a reduction in surface water peak flow rates in accordance with the London Plan from current discharge levels. Thames Water note that this site has reported a single surface water flooding incident in 1995 and would therefore expect the drainage strategy to include features that will reduce the risk of site flooding.

INFORMATIVE: The applicant is advised to contact London Underground Infrastructure Protection in advance of preparation of final design and associated method statements, in particular with regard to: demolition; excavation; construction methods; security; boundary treatment; safety barriers; landscaping and lighting

INFORMATIVE: Written schemes of investigation will need to be prepared and implemented by a suitably qualified archaeological practice in accordance with English Heritage Greater London Archaeology guidelines. They must be approved by the planning authority before any on-site development related activity occurs.

An archaeological field evaluation involves exploratory fieldwork to determine if significant remains are present on a site and if so to define their character, extent, quality and preservation. Field evaluation may involve one or more techniques depending on the nature of the site and its archaeological potential. It will normally include excavation of trial trenches. A field evaluation report will usually be used to inform a planning decision (pre-determination evaluation) but can also be required by condition to refine a mitigation strategy after permission has been granted.

Archaeological excavation is a structured investigation with defined research objectives which normally takes place as a condition of planning permission. It will involve the investigation and recording of an area of archaeological interest including the recovery of artefacts and environmental evidence. Once on-site works have been completed a 'post-excavation assessment' will be prepared followed by an appropriate level of further analysis, publication and archiving.

INFORMATIVE: Adequate storage and collection arrangements for domestic waste and recycling should be in place to service proposed multiple dwellings and proposed business units.

Location of the proposed bin chambers should be easily accessed by waste collection crew and be within a suitable distance in accordance with Council advised above.

Commercial Business must ensure all waste produced on site are disposed of responsibly under their duty of care within Environmental Protection Act 1990. It is for the business to arrange a properly documented process for waste collection from a licensed contractor of their choice. Documentation must be kept by the business and be produced on request of an authorised Council Official under section 34 of the Act. Failure to do so may result in a fixed penalty fine or prosecution through the criminal Court system.

Waste must be properly contained to avoid spillage, side waste and wind blown litter. Waste collection arrangements must be frequent enough to avoid spillage and waste accumulations around the bin area and surrounding land both private and public.

**INFORMATIVE:** The Authority strongly recommends that sprinklers are considered for new developments and major alterations to existing premises, particularly where the proposals relate to schools and care homes. Sprinkler systems installed in buildings can significantly reduce the damage caused by fire and the consequential cost to businesses and housing providers, and can reduce the risk to life. The Brigade opinion is that there are opportunities for developers and building owners to install sprinkler systems in order to save money, save property and protect the lives of occupier. Please note that it is the Authority's policy to regularly advise their elected Members about how many cases there have been where their have recommended sprinklers and what the outcomes of those recommendations were. These quarterly reports to their Members are public documents which are available on their website.

**INFORMATIVE:** Prior to demolition of existing buildings, an asbestos survey should be carried out to identify the location and type of asbestos containing materials. Any asbestos containing materials must be removed and disposed of in accordance with the correct procedure prior to any demolition or construction works carried out.



## Appendix 1 Consultation Responses from internal and external agencies

Stakeholder	Question/Comment	Response
<b>INTERNAL</b>		
Transportation	No objection subject to a S106 agreement securing a car-free development including a financial contribution of £1,000 towards the amendment of the Traffic Management Order, 2 years free membership to a local Car Club and £50 free credit, £3,291 towards commercial cycle parking and conditions covering construction management plan, S278 highway works, delivery and servicing plan, parking and cycling.	Noted and imposed under Conditions 19, 20, 21, 22, 23 and 24. The financial contributions have been secured under the legal agreement.
Conservation	No objection subject to materials and shopfront conditions	Noted and imposed under Conditions 3 and 5
Environmental Health	No objection subject to NOx boilers, community heat boiler, management plan, considerate constructors scheme, demolition and NRMM conditions	Noted and imposed under Conditions 8, 9, 10, 11, 12 and 13.
Cleansing	No objection subject to informatives.	Noted.
Carbon Management	No objection subject to a financial contribution of £22,410 to the Councils carbon offsetting fund and heat network and BREEAM conditions	Noted and imposed under Conditions 14 and 15. The financial contribution has been secured under the legal agreement.
<b>EXTERNAL</b>		
Environment Agency	No comments.	Noted.
Historic England	No objection subject to an archaeological condition.	Noted and imposed under Condition 18.
London Underground	No objection subject to a design and method condition.	Noted and imposed under Condition 17.
Thames Water	No objection subject to a drainage strategy condition and an informative.	Noted and imposed under Condition 16.
London Fire Brigade	No objection subject to an informative.	Noted.
<b>NEIGHBOURING PROPERTIES &amp;</b>	Overdevelopment	The proposed density of 78 units per hectare and 238 habitable rooms per hectare is of an acceptable density

Stakeholder	Question/Comment	Response
<p><b>AMENITY GROUPS</b></p>	<p>Loss and displacement of existing independent businesses and traders including loss of jobs and services</p> <p>Contrary to Haringey’s Sustainable Community Strategy that seek to ‘ensure economic vitality and prosperity is shared by all, through promoting a vibrant economy , increasing skills, raising employment and reducing worklessness’</p> <p>Impact on existing local and independent shops</p> <p>Highway and pedestrian safety from the servicing of the site;</p> <p>Design, scale and bulk of the proposal</p>	<p>for the site as it falls within the appropriate density range as set out in the London Plan for this part of the Borough.</p> <p>The loss of the existing low quality workshop units is acceptable as they will be replaced by higher quality employment generating provision in the form of flexible and affordable B1 workspace.</p> <p>As above.</p> <p>Officers consider the new commercial unit would significantly improve the quality of the retail floorspace on site which in turn enhance the vitality and viability of this commercial section of Archway Road</p> <p>A number of conditions have been suggested requesting details of the construction management plan and servicing of the new commercial unit to ensure it does not prejudice existing road and parking conditions, namely vehicular movements along Archway Road, Causton Road and the local road network generally.</p> <p>The design, bulk and scale of the new development is acceptable as it would considerably enhance the appearance of the building and hence its contribution to the character and appearance of the conservation area.</p>

Stakeholder	Question/Comment	Response
	<p data-bbox="541 342 947 375">Impact on conservation area</p> <p data-bbox="541 597 758 630">Loss of privacy</p> <p data-bbox="541 818 1167 850">Loss of daylight/sunlight and overshadowing</p> <p data-bbox="541 1073 1209 1143">Increased parking pressures on the surrounding roads;</p> <p data-bbox="541 1256 1079 1289">Noise pollution from service deliveries</p>	<p data-bbox="1234 269 2032 483">Although the proposals will cause some visual harm to the character and appearance of the conservation area the harm is considered to be less than substantial. This harm has been given considerable weight by officers but it is outweighed by the significant heritage benefits of the scheme as a whole.</p> <p data-bbox="1234 524 2032 704">The siting and orientation of the habitable room windows proposed have been carefully sited so as to maintain acceptable levels of privacy currently enjoyed by occupiers living at 2 Causton Road (west) and 203 Archway Road (north).</p> <p data-bbox="1234 745 2032 959">The proposed development would satisfy the daylight and sunlight BRE recommendations in maintaining an acceptable level of living conditions currently enjoyed by habitants of the adjacent properties at Nos. 187 to 189 Archway Road; 2 Causton Road; and 203 Archway Road</p> <p data-bbox="1234 1000 2032 1180">A 'car free' development will be secured under the legal agreement meaning future occupiers of the new development will not be allowed to apply for resident permits. An acceptable level of cycling parking has been provided.</p> <p data-bbox="1234 1188 2032 1328">A delivery and servicing management plan (DSP) on occupation of the development is sought by condition, in the interest of minimising impacts on local amenity and traffic in the adjoining road network</p> <p data-bbox="1234 1369 2032 1399">Details of the construction management plan will be</p>

Stakeholder	Question/Comment	Response
	<p>Disturbance caused by construction vehicles</p> <p>Lack of affordable housing</p> <p>Flood risk</p>	<p>sought by condition.</p> <p>The applicant has agreed to make a off-site affordable housing payment of £255,000.</p> <p>The site falls within flood risk zone 1 with low risk to flooding and a drainage condition has been sought as recommended by Thames Water.</p>

## Appendix 2: Plans and images



01 2-4 Causton Road, residential adjacent to the site.



02 203-205 Archway Road, residential adjacent to the site



03 Highgatehill Murugan Temple



04 Street front on opposite side of Archway Road



Key Plan

Surrounding context



05 Grand Parade no.'s 162 - 198 Archway Rd showing locally Listed Buildings.



01 Site photograph taken from Archway Road looking towards site



03 Site photograph at rear of site



05 View of flat roof above commercial space at ground.



02 Site photograph taken from Causton Road looking towards site



04 Photograph taken from external deck looking to internal courtyard



06 Photograph of the rear facade of 191 Archway Road



Key Plan  
Application site



07 Photograph taken from external deck looking to internal courtyard



Richardson of Highgate basement area



Richardson of Highgate shopfloor looking out towards Archway Road



Access down to Richardson of Highgate basement area



Richardson of Highgate shopfloor



Richardson of Highgate basement area looking up through pavement lights



Richardson of Highgate shopfloor from Archway Road entrance

Existing basement and ground floor photos



Richardson of Highgate unoccupied first floor shopfloor



View of staircase leading to HMO studio apartments at 3rd floor level



HMO private kitchen facilities



Richardson of Highgate unoccupied shopfloor looking out towards Archway Road through the bay window



View of second floor HMO accommodation overlooking Archway Road



HMO private kitchen facilities



Richardson of Highgate unoccupied shopfloor looking out towards Causton Road



View of second floor HMO accommodation



View of third floor HMO accommodation overlooking Archway Road

Existing first, second and third floor photos

For Sub Committee

OFFREPC  
Officers Report





Existing site location plan



Existing site plan



Existing ground floor plan



Existing first floor plan

For Sub Committee

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Officers Report



Existing second floor plan

For Sub Committee

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Officers Report

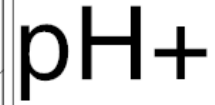


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Revisions  
 1 18/10 PLANNING 05/12

- KEY
- Class A1: Retail
  - Class B1a: Business - office
  - Class B1c: Business - light industrial
  - Class C4: HMO's
  - Class D1: Non-residential institutions
  - Class D2: Assembly & Leisure
  - Garage / Car Parking

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Project  
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Drawing  
 Existing  
 Third Floor Plan

Scale at A1 (A3)	Date
1:100 (1:200)	August 2015

Job Number	Drawing No.	Rev
499	0013-GA	1

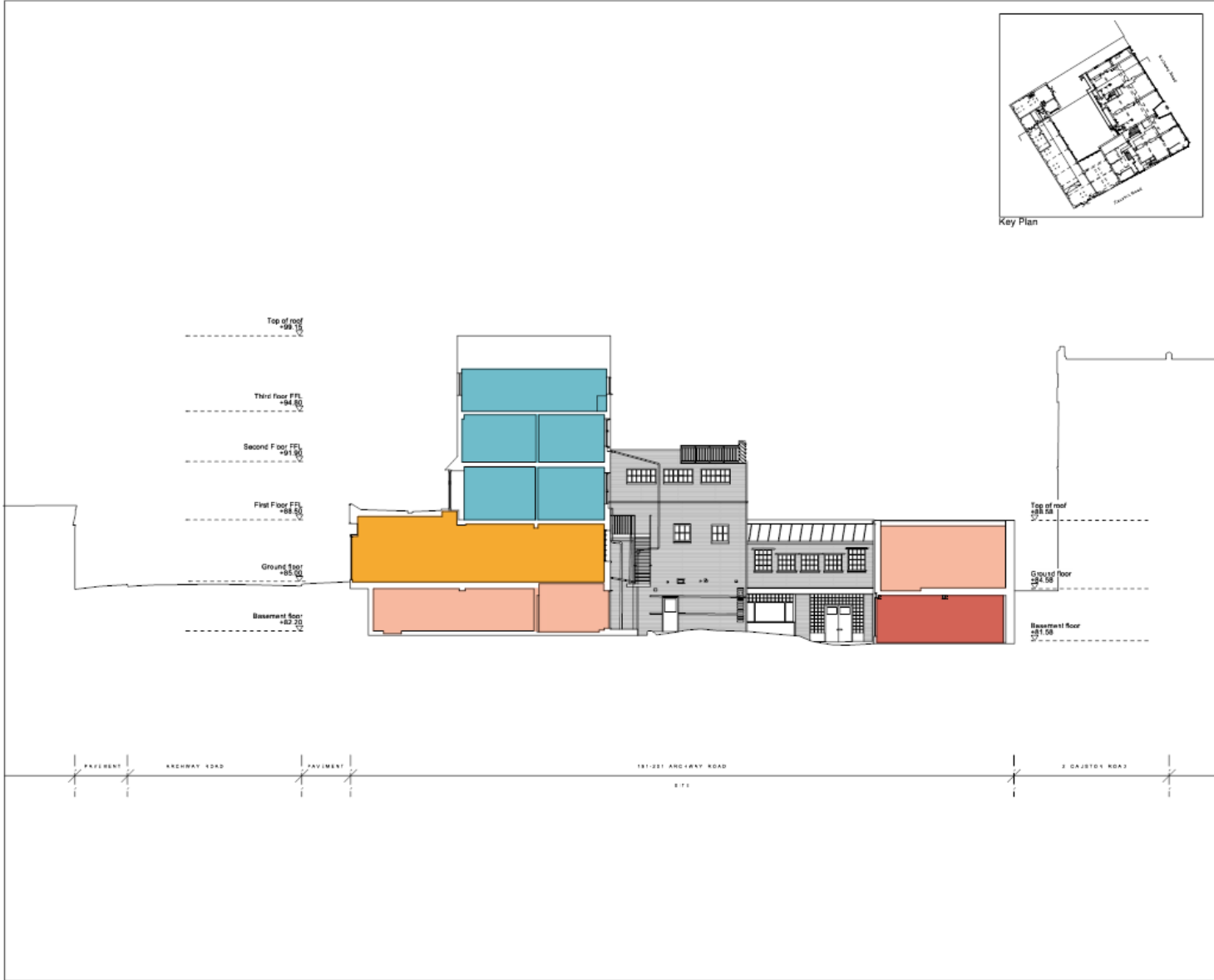
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Existing third floor plan



Existing basement floor plan

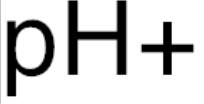


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Revisions  
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Drawing  
Existing  
Section AA

Scale at A1 (A3) 1:100 (1:200)	Date August 2015
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Job Number 499	Drawing No. 0030-GA	Rev 1
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Drawn by DR	Checked by APa
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Existing section AA





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1 18/10/15 PLANNING SUBJ

KEY

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- Class B1c: Business - light industrial
- Class C4: HMO's
- Class D1: Non-residential institutions
- Class D2: Assembly & Leisure
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Drawing  
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Section BB

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Date  
August 2015

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Drawing No. Rev  
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Status

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Existing section BB

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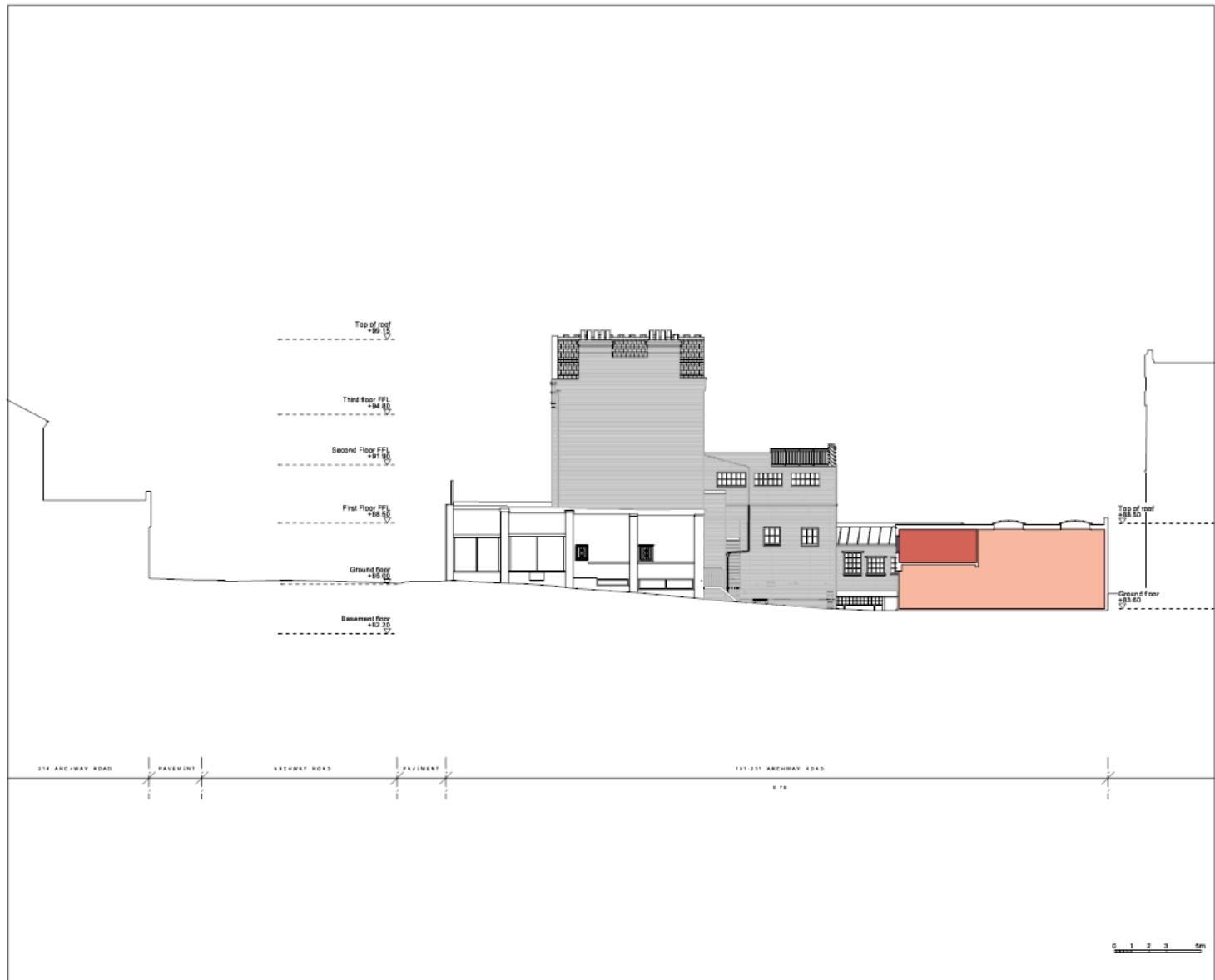
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Existing NE elevation



Existing NW elevation

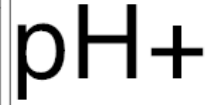


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1	18/10 *LAWING BDU2

- KEY
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Drawing  
Existing  
South East Elevation

Scale at A1 (A3)	Date
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Job Number	Drawing No.	Rev
499	0042-GA	1

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Existing SE elevation

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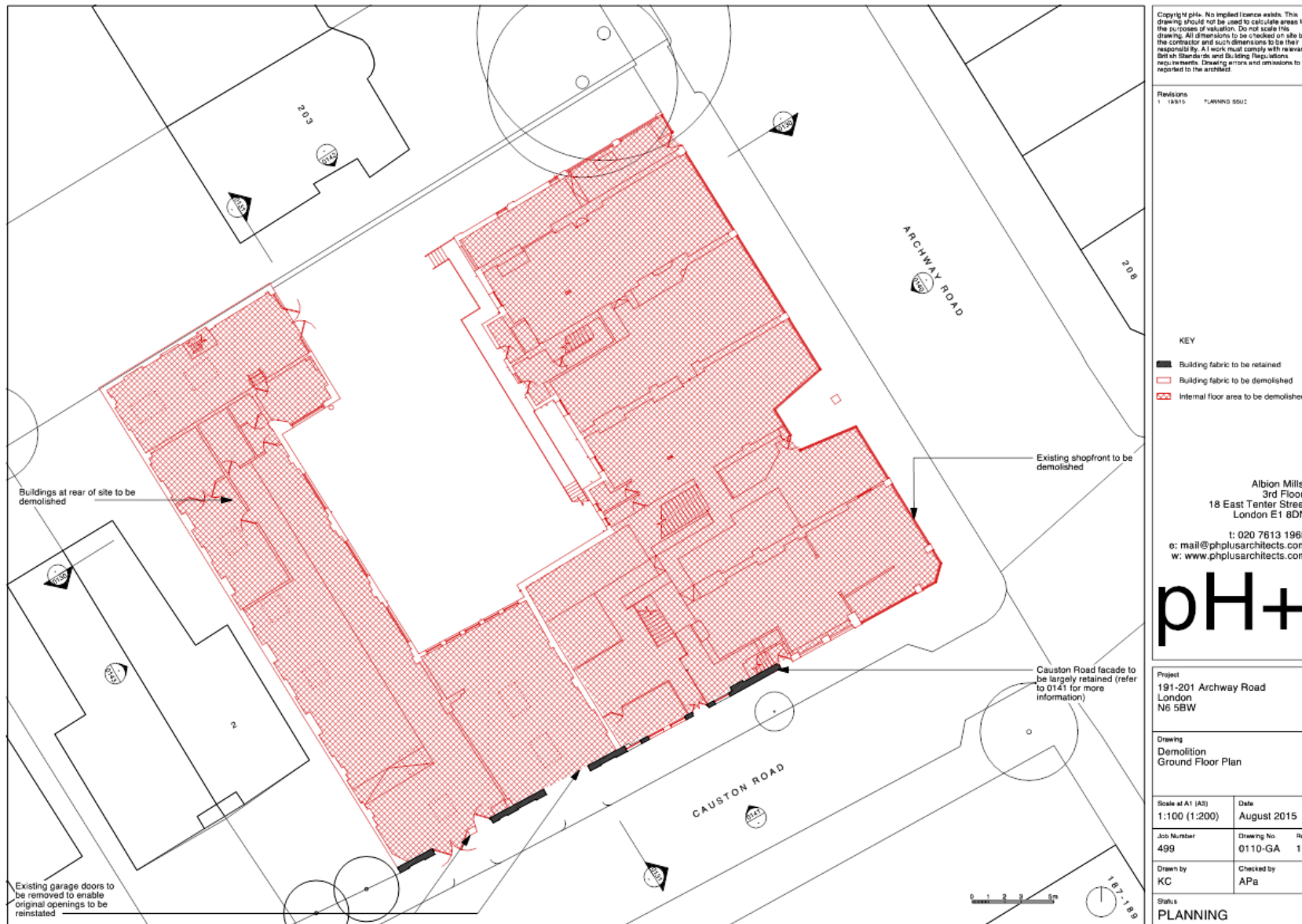
Existing SW elevation



Proposed site location plan

For Sub Committee

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Demolition ground floor plan



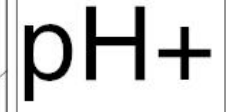
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Revisions	
1	18/01/15 "PLANNING ISSUE"

KEY

	Building fabric to be retained
	Building fabric to be demolished
	Internal floor area to be demolished

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Drawing  
Demolition  
First Floor Plan

Scale at A1 (A3)	Date
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Job Number	Drawing No.	Rev
499	0111-GA	1

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Status  
**PLANNING**

Demolition first floor plan





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Revisions  
1 19/10 2015 PLANNING 05/12

KEY  
 ■ Building fabric to be retained  
 □ Building fabric to be demolished  
 ▨ Internal floor area to be demolished

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Drawing  
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 Second Floor Plan

Scale at A1 (A3)  
 1:100 (1:200) Date  
 August 2015

Job Number  
 499 Drawing No.  
 0112-GA Rev  
 1

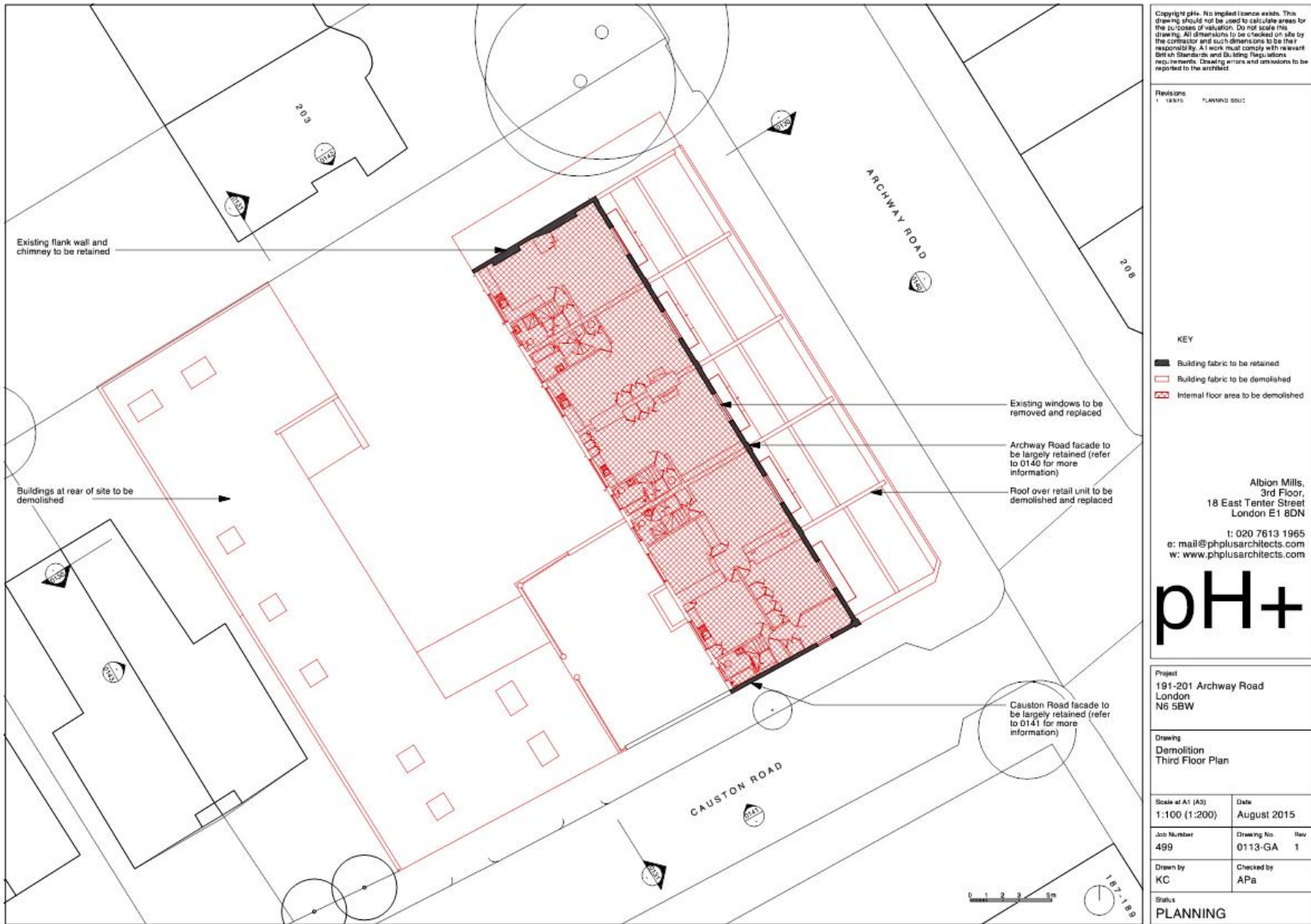
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Demolition second floor plan

For Sub Committee

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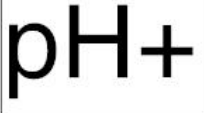
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1	19/05/15 PLANNING SUB

KEY

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Drawing  
Demolition  
Third Floor Plan

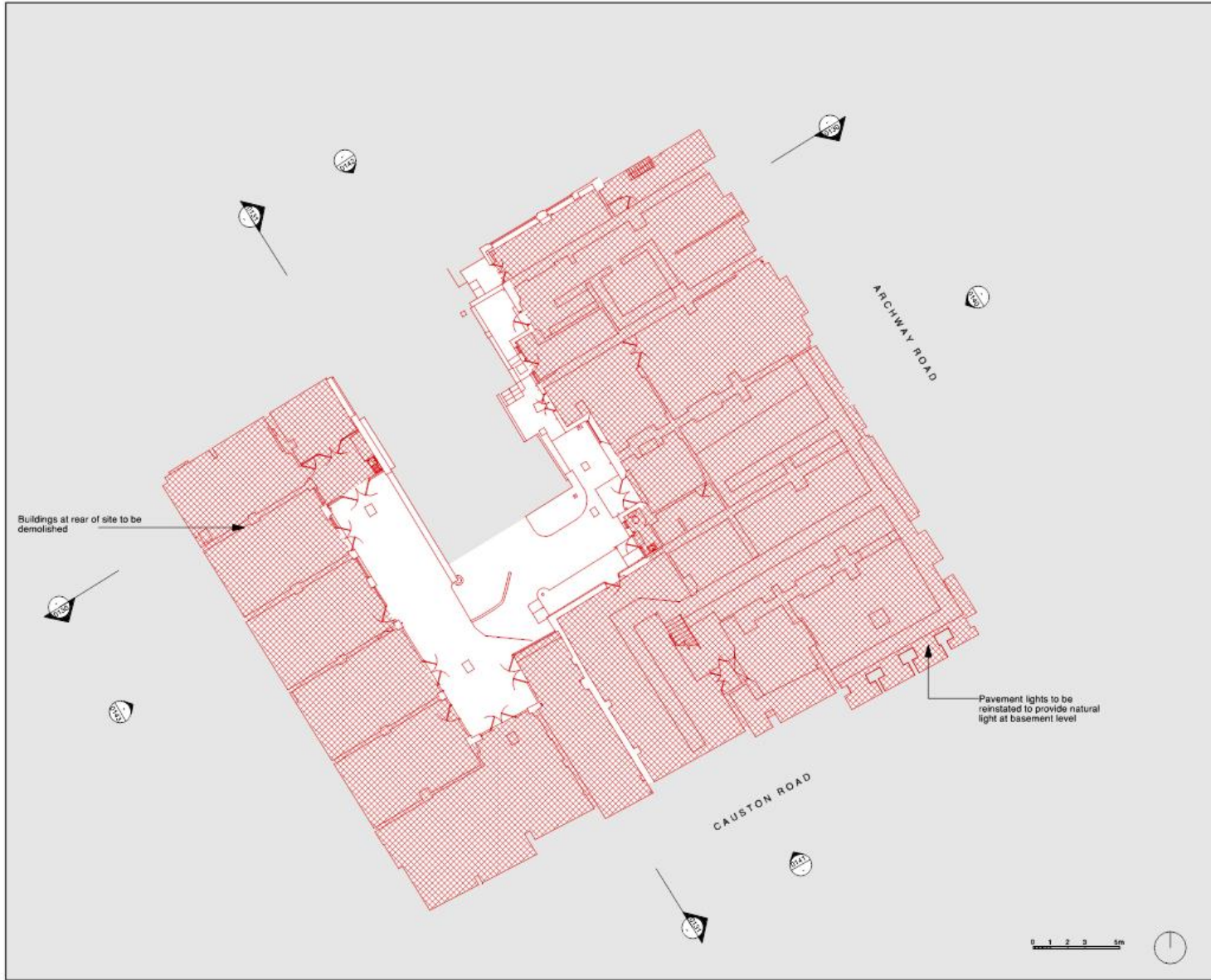
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Demolition third floor plan



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Drawing  
 Demolition  
 Basement Floor Plan

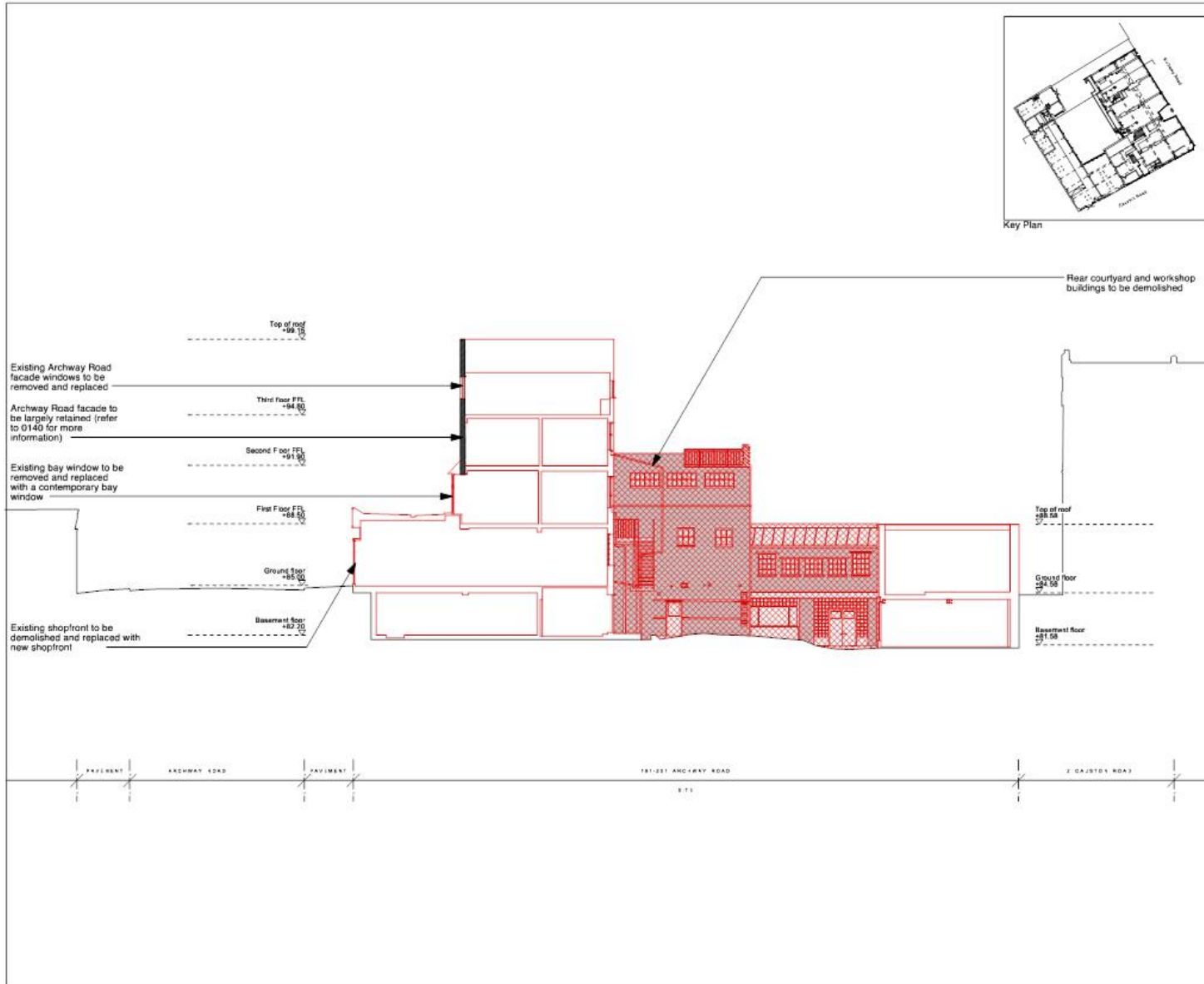
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Demolition basement floor plan

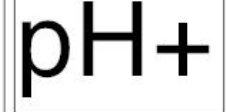


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1 18/15 PLANNING 05/15

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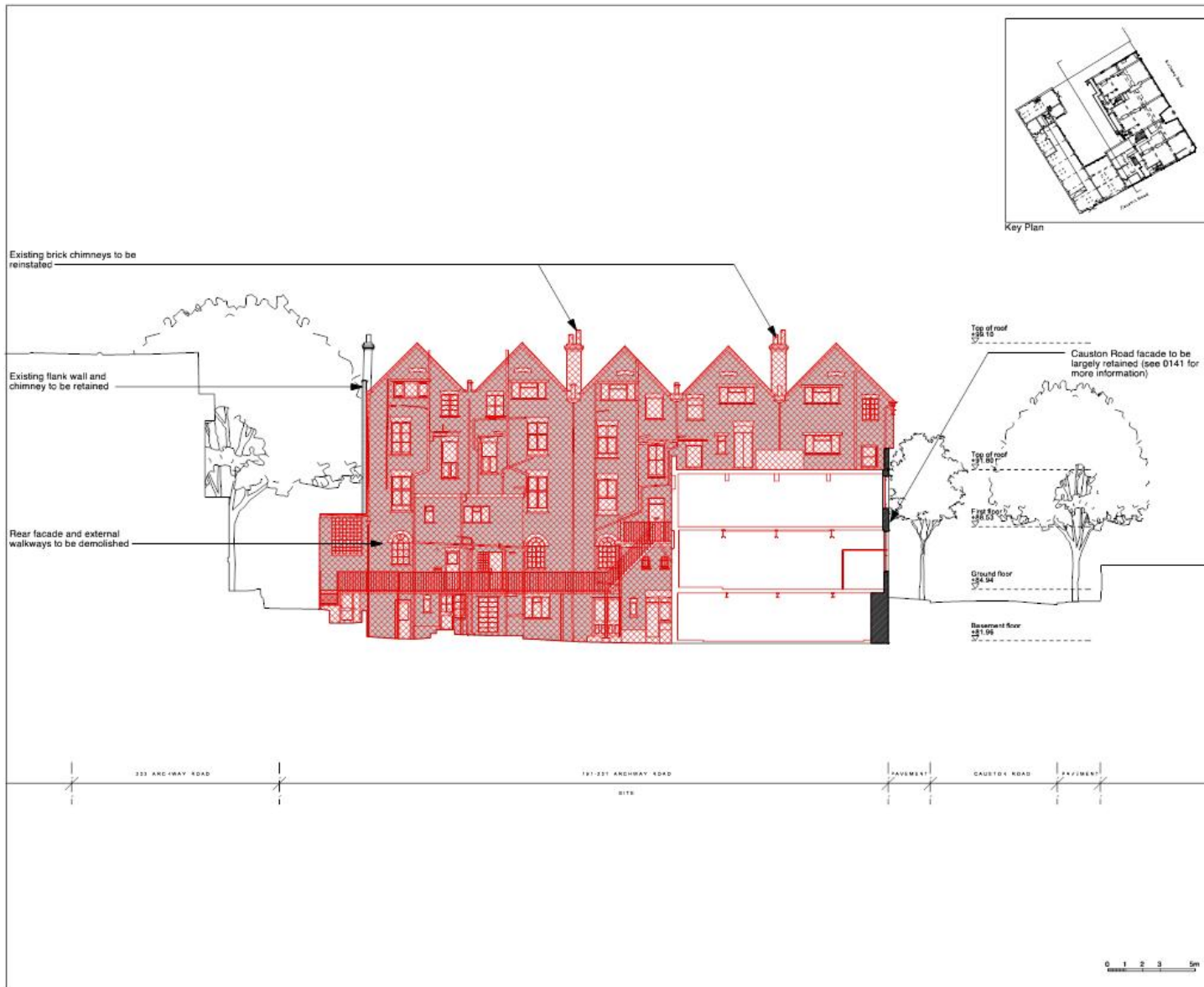
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Drawing  
 Demolition  
 Section AA

Scale at A1 (A3)	Date
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499	0130-GA 1
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Demolition section AA



Demolition section AA

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1 18/05 2015 PLANNING ISSUE

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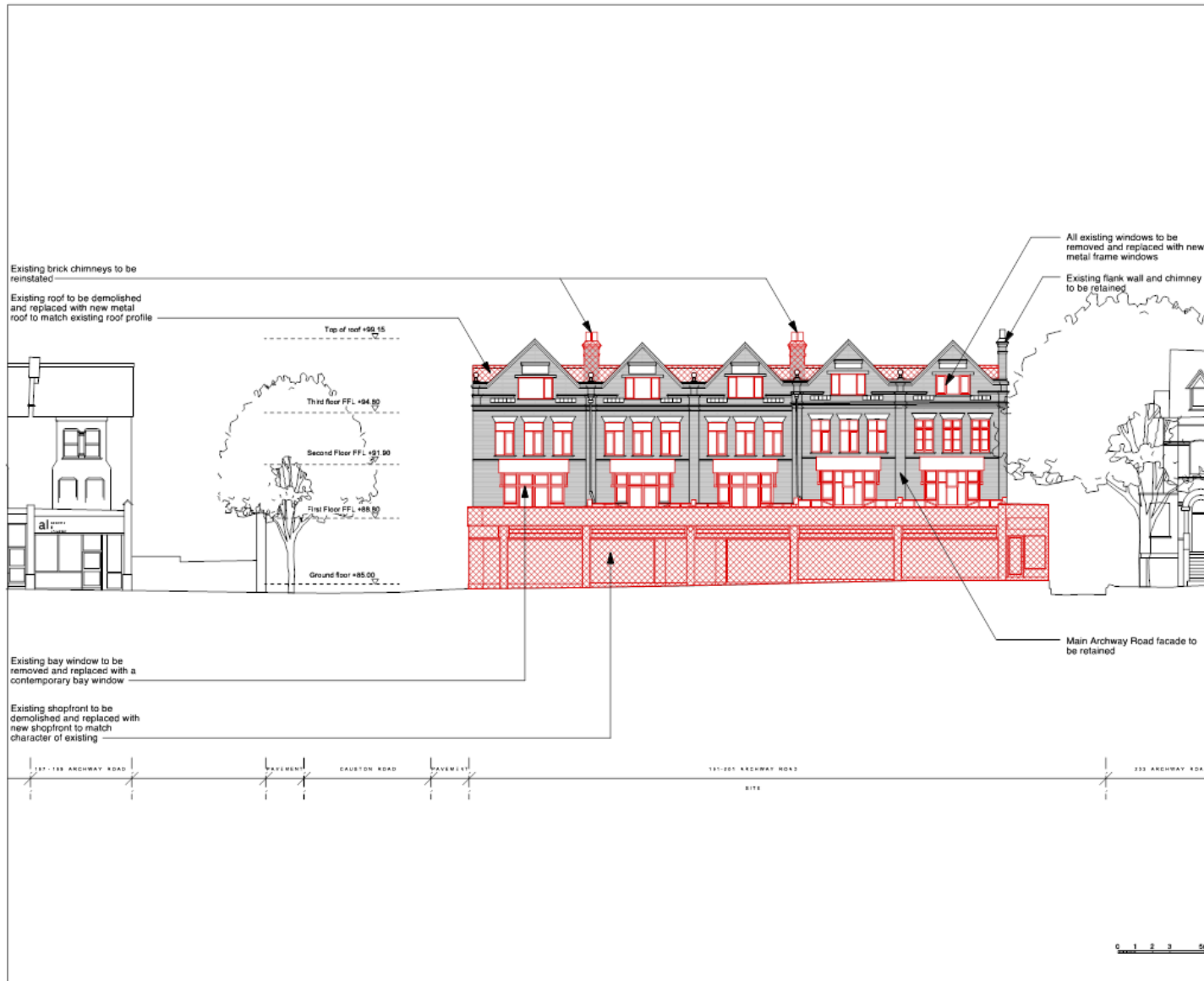
Drawing  
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 Section BB

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Revisions  
1 18/15 PLANNING 0501

KEY

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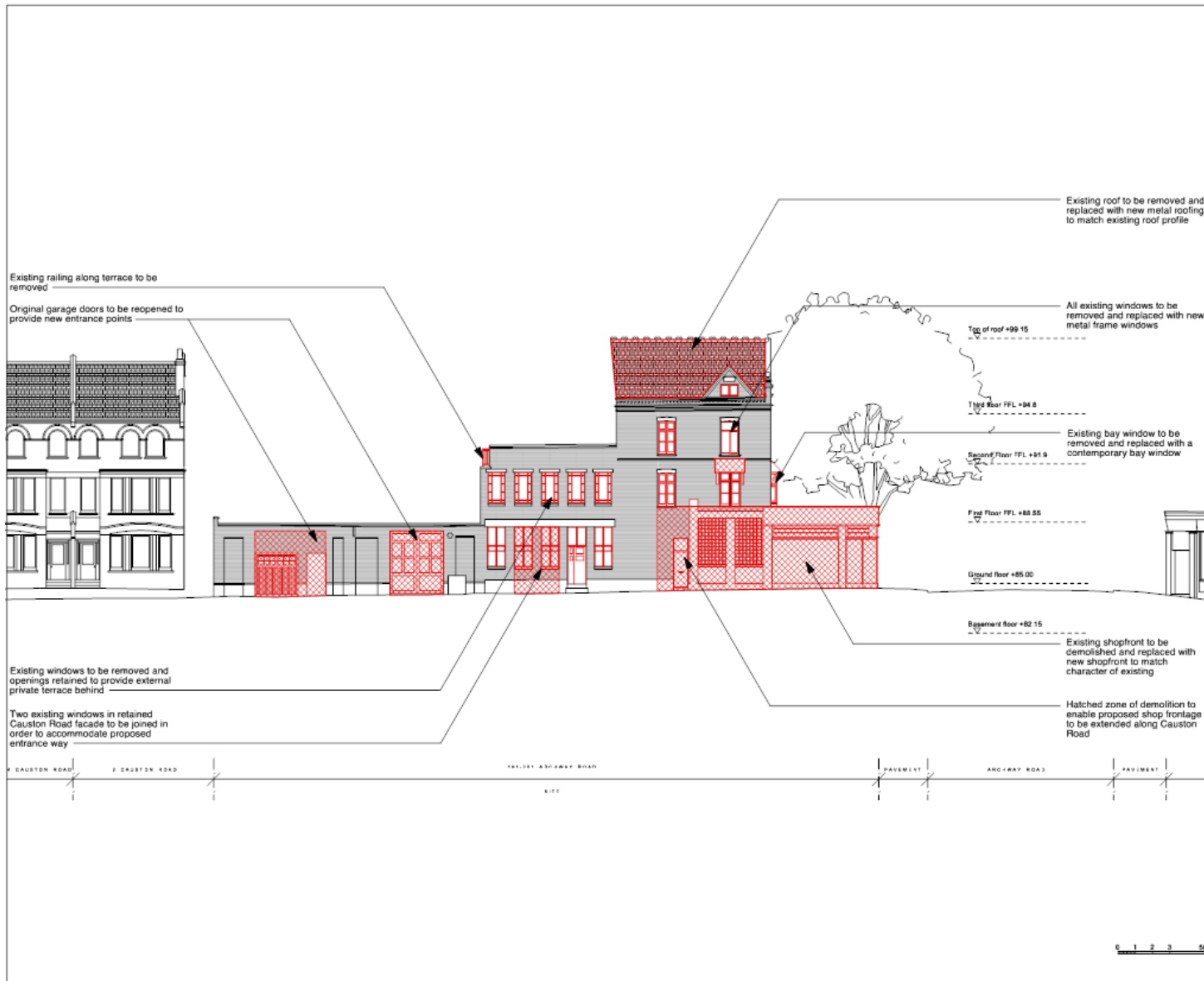
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Project  
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Drawing  
Demolition  
North-East Elevation

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Job Number	Drawing No. Rev
499	0140-GA 1
Drawn by	Checked by
KC	APa
Status	
PLANNING	

Demolition NE elevation

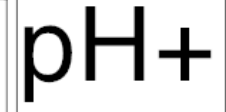


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Revisions	
1	18/01/15 J. JAMES/BSJ

- KEY
- Building fabric to be retained
  - Building fabric to be demolished
  - External wall area to be demolished

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Project  
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Drawing  
Demolition  
North West Elevation

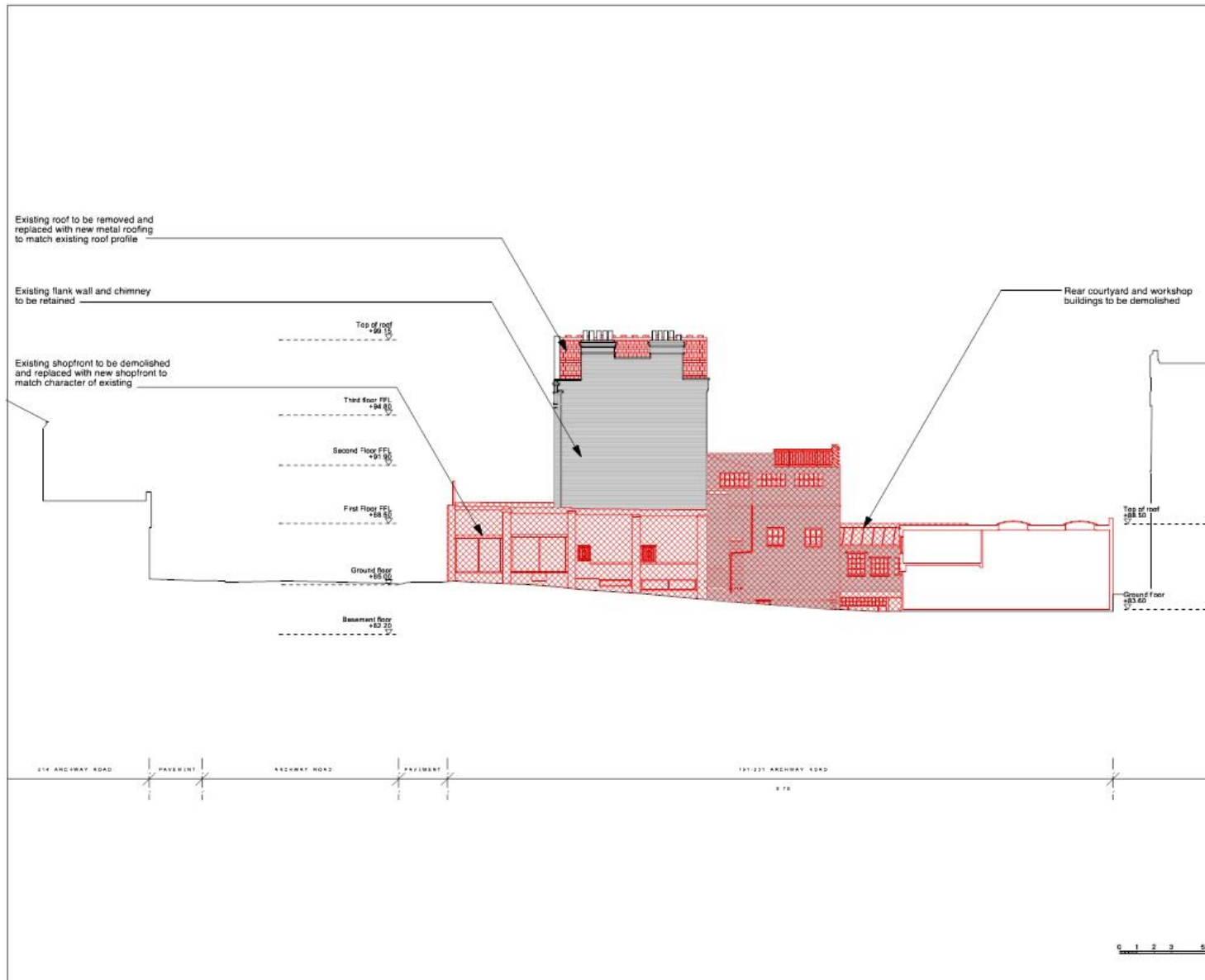
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Job Number	Drawing No.	Rev
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Status  
**PLANNING**

Demolition NW elevation



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Revisions  
1 19.8.15 PLANNING 8502

KEY  
 ■ Building fabric to be retained  
 □ Building fabric to be demolished  
 ▨ External wall area to be demolished

Albion Mills,  
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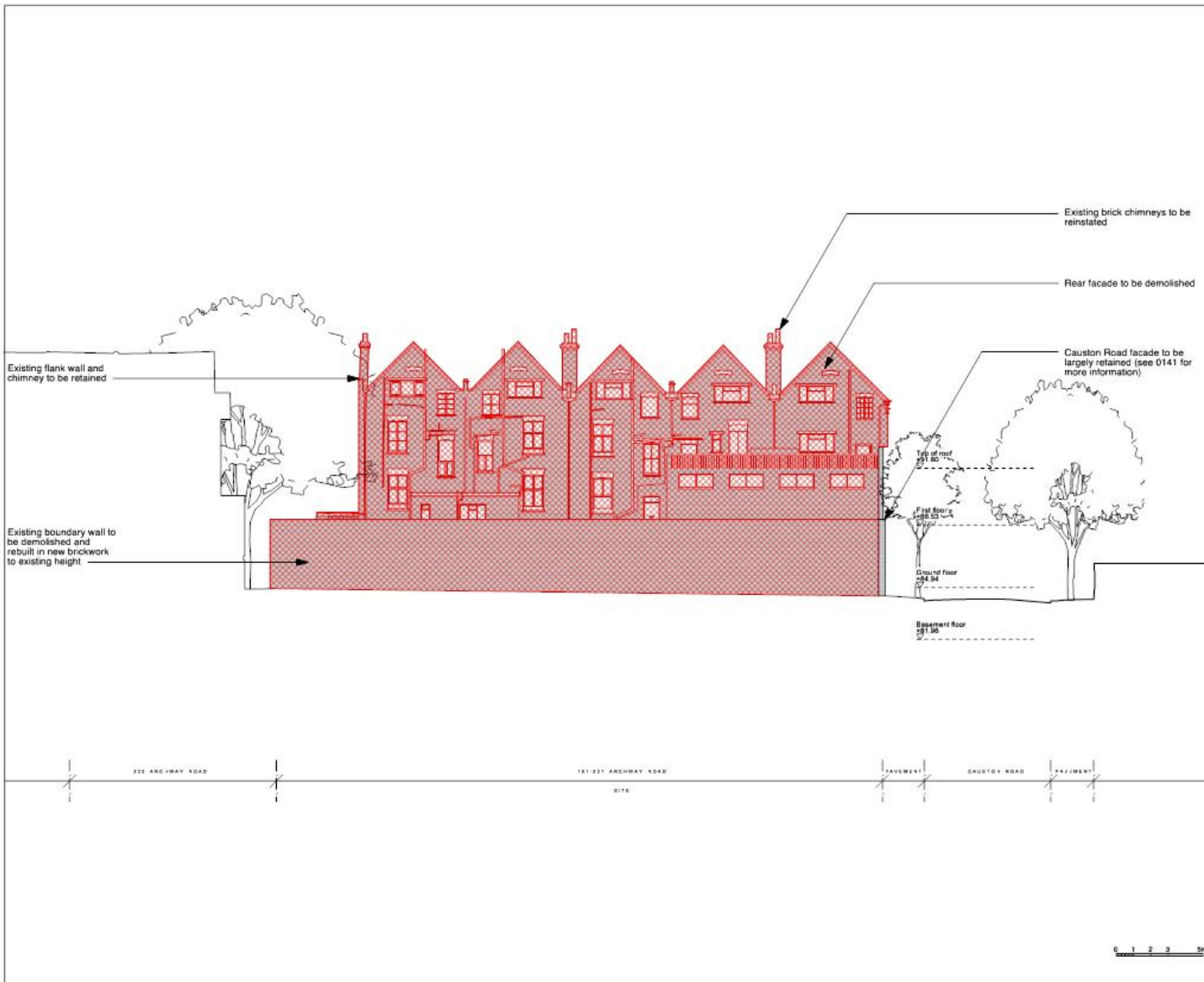
Project  
191-201 Archway Road  
London  
N6 5BW

Drawing  
Demolition  
South East Elevation

Scale at A1 (A3)	Date	
1:100 (1:200)	August 2015	
Job Number	Drawing No.	Rev
499	0142-GA	1
Drawn by	Checked by	
KC	APa	
Status		
PLANNING		

Demolition SE elevation





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Revisions  
1 18/15 PLANNING SDU

KEY  
 ■ Building fabric to be retained  
 □ Building fabric to be demolished  
 ▨ External wall area to be demolished

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Project  
181-201 Archway Road  
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Drawing  
Demolition  
South West Elevation

Scale at A1 (A3) 1:100 (1:200)	Date August 2015
Job Number 499	Drawing No. Rev 0143-GA 1
Drawn by KC	Checked by APa

Status  
PLANNING

Demolition SW elevation



Proposed ground floor plan



Proposed first floor plan

For Sub Committee

OFFREPC  
 Officers Report



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Revision	Date	Description
1	01/11/14	
2	14/11/14	
3	31/11/14	
4	16/12/14	
5	24/01/15	
6	31/01/15	
7	18/01/15	PLANNING BOARD

KEY

- Existing walls
- Class C3: 1 Bed Residential units
- Class C3: 2 Bed Residential units
- Class C3: 3 Bed Residential units
- Class A1: Retail
- Flexible Class B1/D2
- Garage / Car Parking
- Circulation
- Residential Amenity

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Project  
191-201 Archway Road  
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N6 5BW

Drawing  
Proposed  
Second Floor Plan

Scale at A1 (A3)	Date
1:100 (1:200)	July 2015

Job Number	Drawing No	Rev
499	0202-GA	7

Drawn by	Checked by
JB	APa

Status  
PLANNING

Proposed second floor plan

For Sub Committee

OFFREPC  
Officers Report



Proposed third floor plan

For Sub Committee

OFFREPC  
 Officers Report

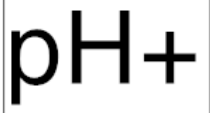


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Rev. no.	DATE	DESCRIPTION
1	25/07/15	ISSUE FOR DEVELOPMENT
2	18/08/15	PLANNING SUBMISSION

- KEY**
- Existing walls
  - Class C3: 1 Bed Residential units
  - Class C3: 2 Bed Residential units
  - Class C3: 3 Bed Residential units
  - Class A1: Retail
  - Flexible Class B1/D2
  - Garage / Car Parking
  - Circulation
  - Residential Amenity

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**Project**  
191-201 Archway Road  
London  
N6 5BW

**Drawing**  
Proposed  
Roof Plan

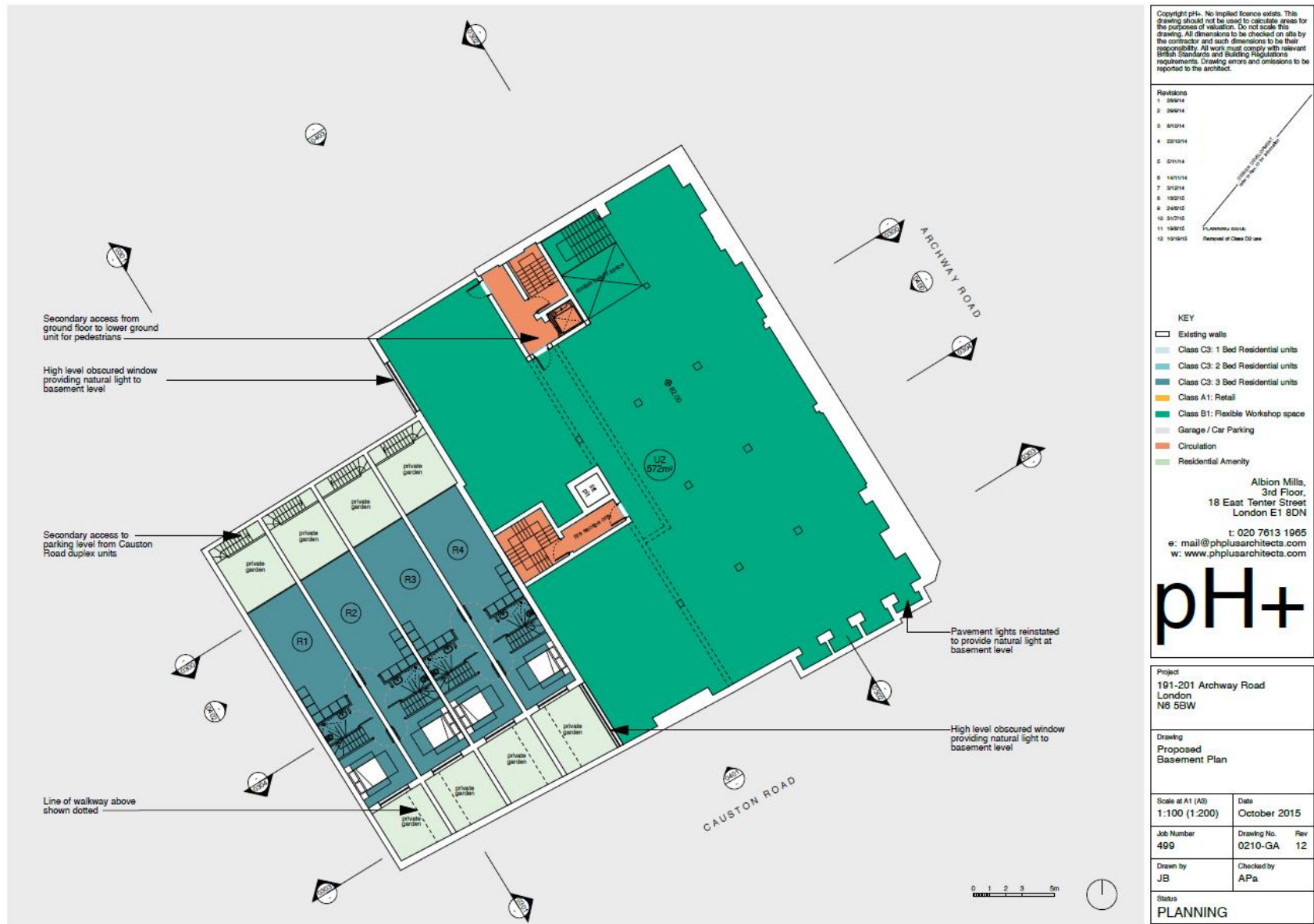
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1:100 (1:200)	July 2015

Job Number	Drawing No.	Rev
499	0204-GA-2	2

Drawn by	Checked by
JB	APa

Stage  
**PLANNING**

Proposed roof plan



Proposed basement floor plan

For Sub Committee

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Officers Report



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Revisions	DESIGN DEVELOPMENT
1 20/15	WORK FOR PERMIT
2 13/16	PLANNING SUBMIT

KEY

- Class C3: 1 Bed Residential units
- Class C3: 2 Bed Residential units
- Class C3: 3 Bed Residential units
- Class A1: Retail
- Flexible Class B1/D2
- Garage / Car Parking
- Circulation
- Residential Amenity

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Project  
191-201 Archway Road  
London  
N6 5BW

Drawing  
Proposed  
Section AA

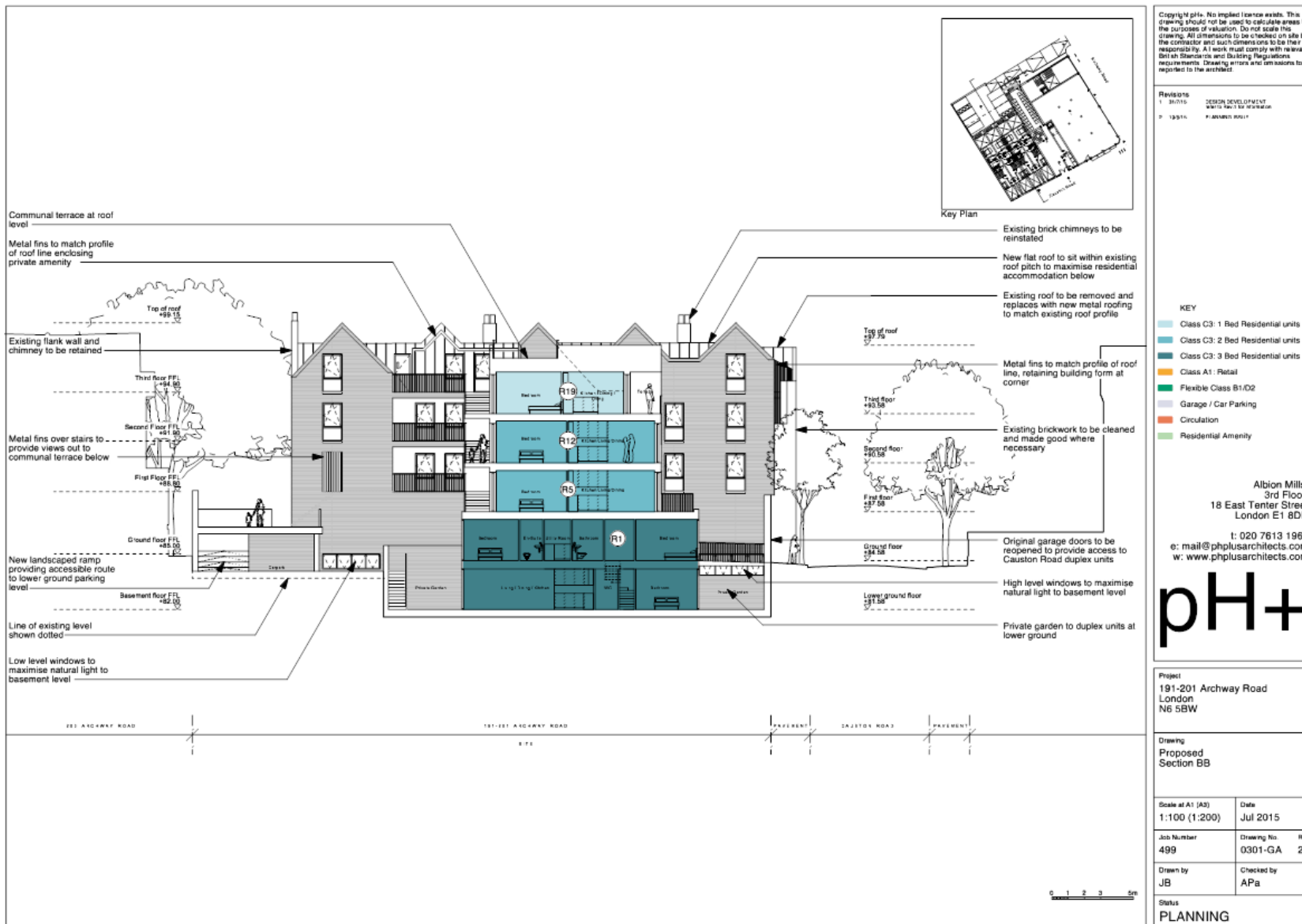
Scale at A1 (A3)	Date	
1:100 (1:200)	Jul 2015	
Job Number	Drawing No.	Rev
499	0300-GA	2
Drawn by	Checked by	
JB	APA	
Status	PLANNING	

Proposed section AA

For Sub Committee

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Revisions

1	31/01/15	DESIGN DEVELOPMENT	PH+ ARCHITECTS
2	19/01/16	PLANNING PERMIT	PH+ ARCHITECTS

KEY

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<span style="color: darkblue;">■</span>	Class C3: 3 Bed Residential units
<span style="color: orange;">■</span>	Class A1: Retail
<span style="color: green;">■</span>	Flexible Class B1/D2
<span style="color: grey;">■</span>	Garage / Car Parking
<span style="color: red;">■</span>	Circulation
<span style="color: lightgreen;">■</span>	Residential Amenity

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Drawing  
Proposed  
Section BB

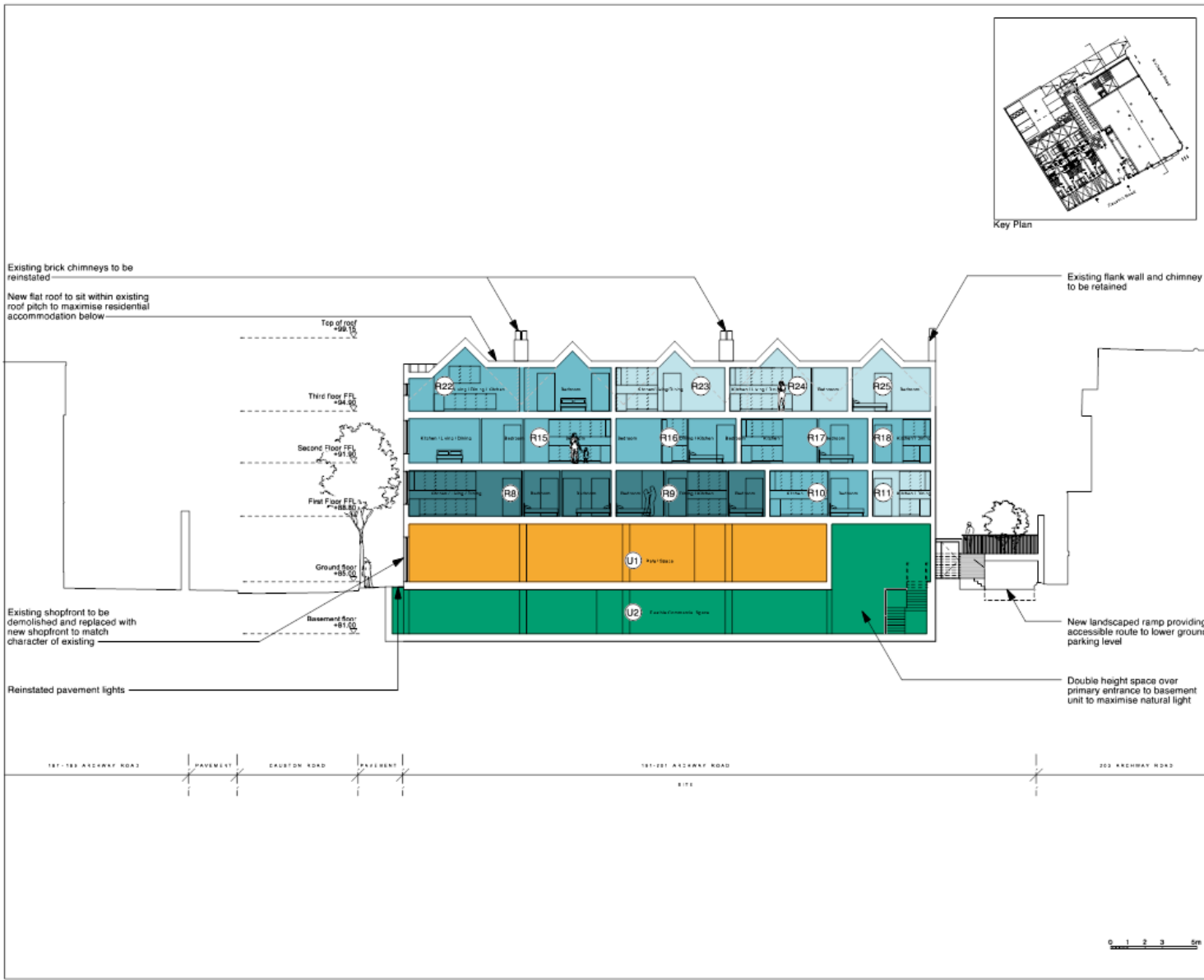
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Job Number 499	Drawing No. 0301-GA	Rev 2
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Drawn by JB	Checked by APa
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Status  
**PLANNING**

Proposed section BB

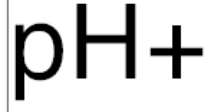


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Revisions	
1	30/05/15 DESIGN DEVELOPMENT
2	13/06/15 PLANNING SUBMIT

- KEY
- Class C3: 1 Bed Residential units
  - Class C3: 2 Bed Residential units
  - Class C3: 3 Bed Residential units
  - Class A1: Retail
  - Flexible Class B1/D2
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  - Circulation
  - Residential Amenity

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Project  
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London  
N6 5BW

Drawing  
Proposed  
Section CC

Scale at A1 (A3)	Date	
1:100 (1:200)	Jul 2015	
Job Number	Drawing No.	Rev
499	0302-GA	2
Drawn by	Checked by	
JB	APa	
Status	PLANNING	

Proposed section CC

For Sub Committee

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Officers Report

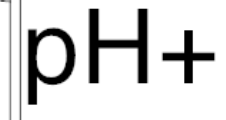


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Revisions	DESIGN DEVELOPMENT
1 30/05/15	24/05/15 BY PH+
2 12/06/15	01/JANUARY 2017

- KEY
- Class C3: 1 Bed Residential units
  - Class C3: 2 Bed Residential units
  - Class C3: 3 Bed Residential units
  - Class A1: Retail
  - Flexible Class B1/D2
  - Garage / Car Parking
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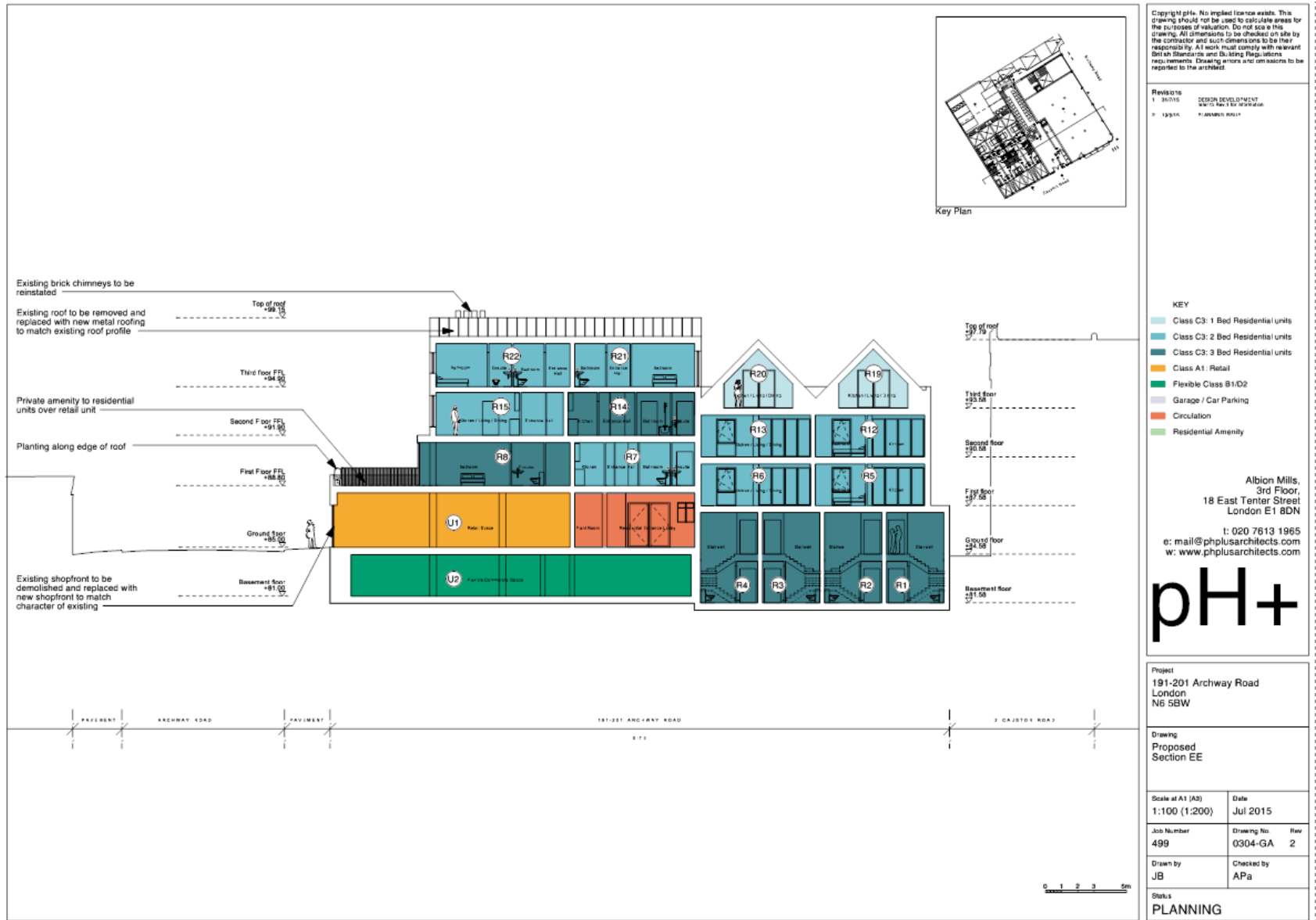
Project  
191-201 Archway Road  
London  
N6 5BW

Drawing  
Proposed  
Section DD

Scale at A1 (A3)	Date
1:100 (1:200)	Jul 2015
Job Number	Drawing No. Rev
499	0303-GA 2
Drawn by	Checked by
JB	APa

Status  
PLANNING

Proposed section DD

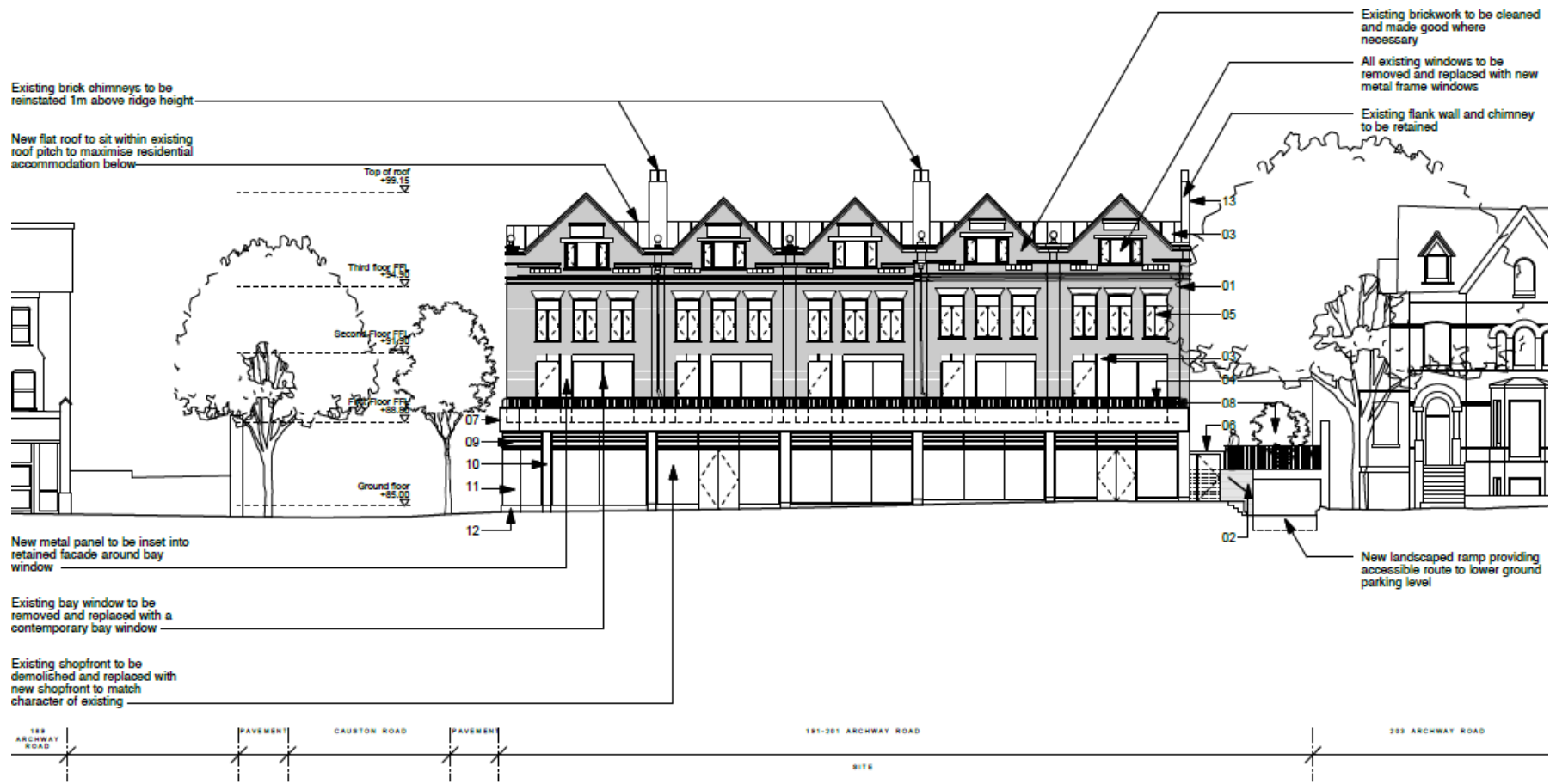


Proposed section EE

For Sub Committee

OFFREPC  
 Officers Report

KEY	
01	Existing red brick
02	Proposed brick to compliment existing
03	Natural metal roofing / cladding to evoke tone of red brick
04	Minimal metal balustrade
05	Metal framed glazing
06	Metal fins
07	Painted timber fascia with signage
08	Planting in front of railing
09	Obscured clerestory in line with glazed shopfront
10	New dark stone pilaster
11	New glazed shopfront window
12	New dark stone stall riser
13	New brick chimney stack



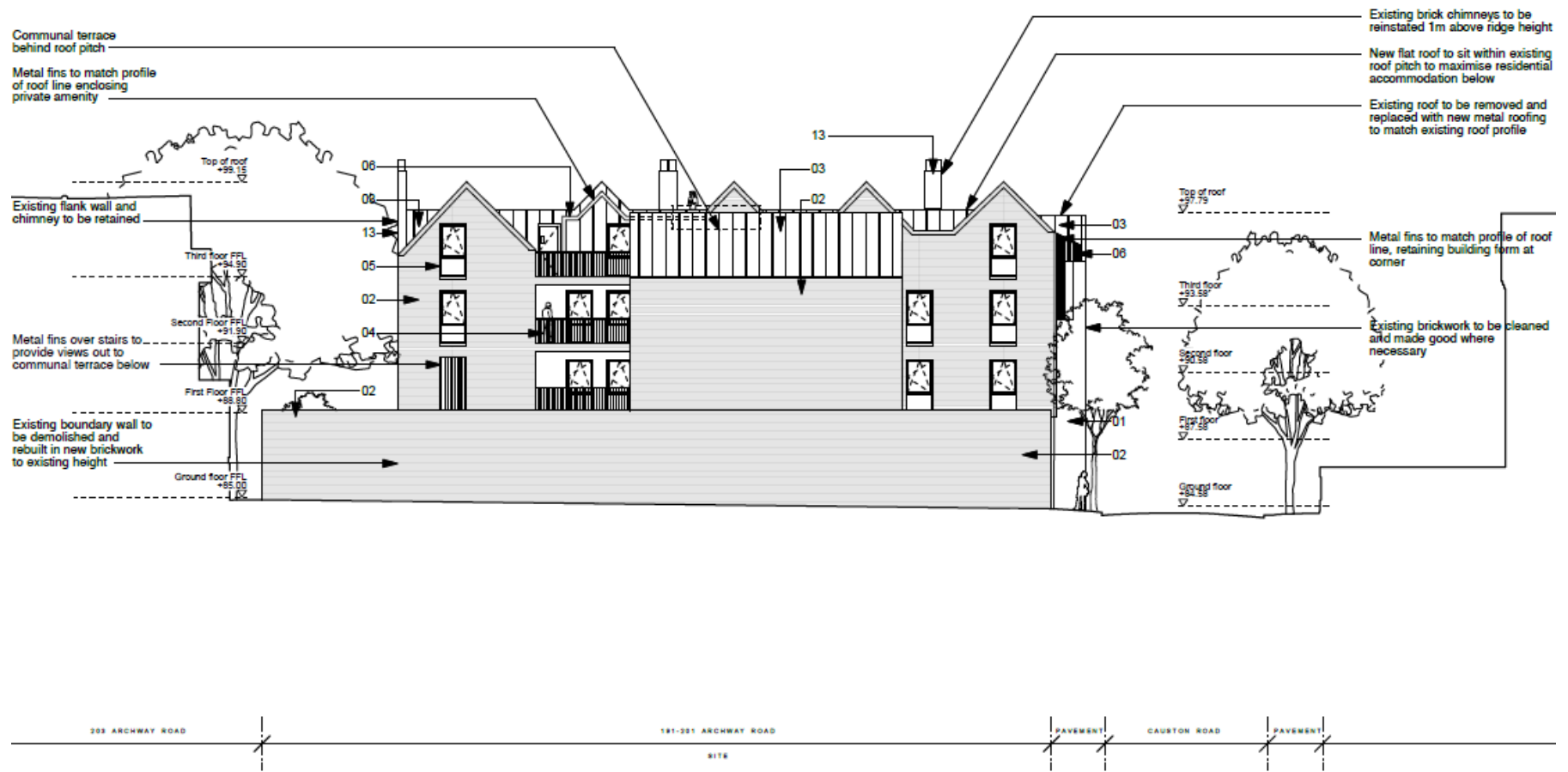
Proposed NE elevation

KEY	
01	Existing red brick
02	Proposed brick to compliment existing
03	Natural metal roofing / cladding to evoke tone of red brick
04	Minimal metal balustrade
05	Metal framed glazing
06	Metal fins
07	Painted timber fascia with signage
08	Planting in front of railing
09	Obscured clerestory in line with glazed shopfront
10	New dark stone pilaster
11	New glazed shopfront window
12	New dark stone stall riser
13	New brick chimney stack



Proposed NW elevation

KEY	
01	Existing red brick
02	Proposed brick to compliment existing
03	Natural metal roofing / cladding to evoke tone of red brick
04	Minimal metal balustrade
05	Metal framed glazing
06	Metal fins
07	Painted timber fascia with signage
08	Planting in front of railing
09	Obscured clerestory in line with glazed shopfront
10	New dark stone pilaster
11	New glazed shopfront window
12	New dark stone stall riser
13	New brick chimney stack

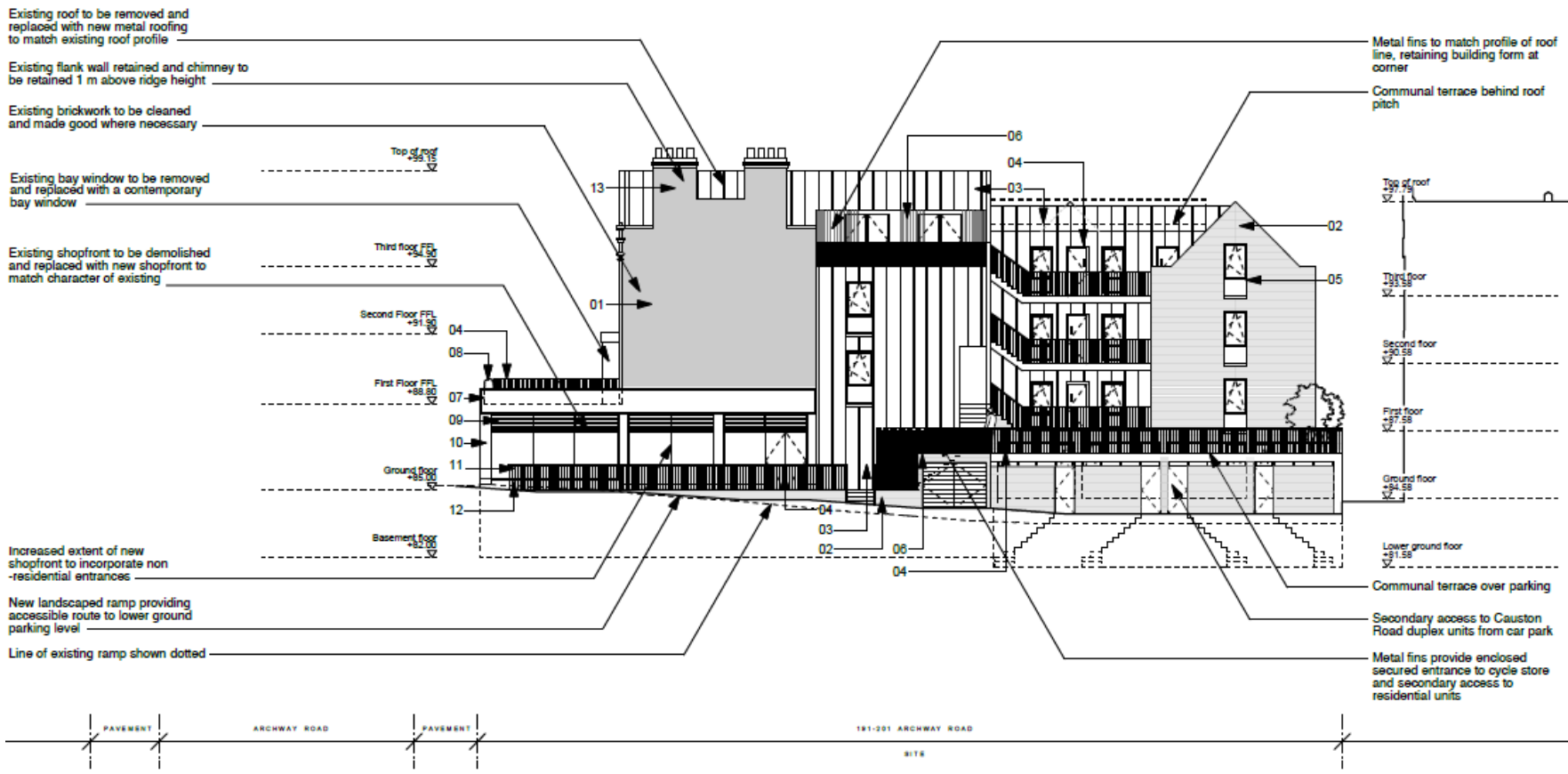


Proposed SW elevation

For Sub Committee

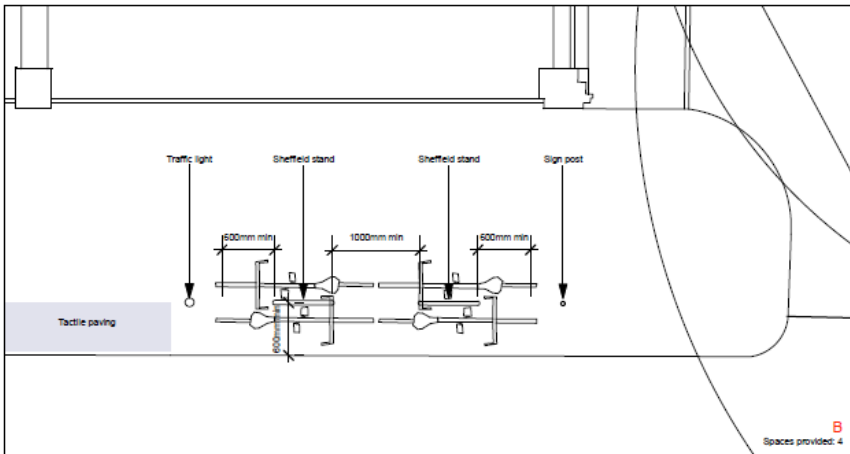
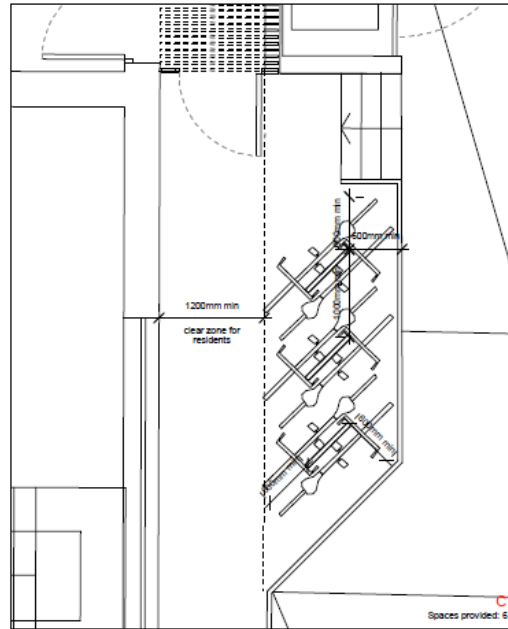
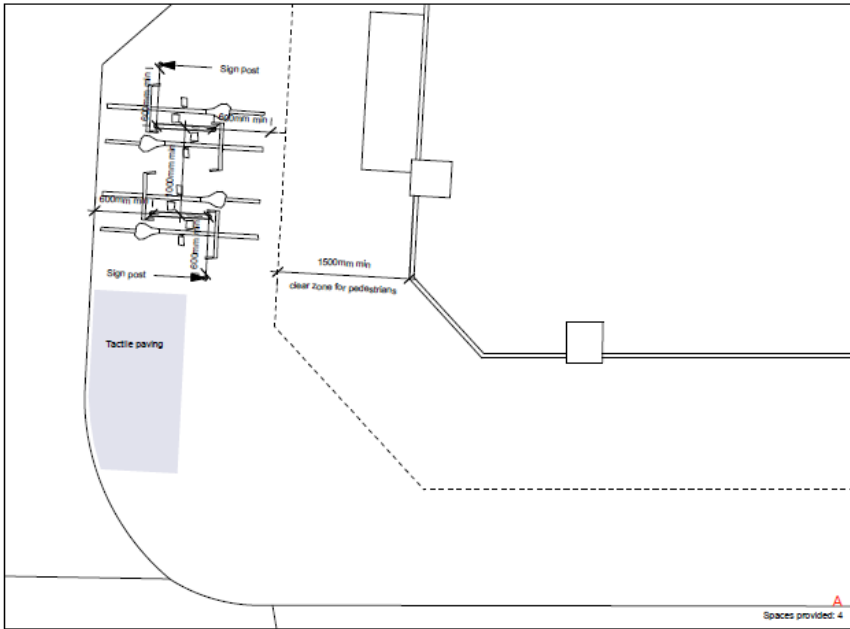
OFFREPC  
Officers Report

KEY	
01	Existing red brick
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13	New brick chimney stack



Proposed SE elevation





Proposed cycling provision

Causton Street - Existing



Archway Road - Existing



Causton Street - New



Archway Road - New



Archway Road - Existing



Rear - Existing



Archway Road - New



Rear - New





CGI view from Causton Road



CGI view from Archway Road

## Appendix 3: Quality Review Panel

### CONFIDENTIAL

#### 1. Project name and site address

191-201 Archway Road, London, N6 5BN

#### 2. Presenting team

Joe Gerrard	Archway Apartments Ltd
Michael Gerrard	Archway Apartments Ltd
Andy Puncher	pH+ Architects
Annie Pace	pH+ Architects
Diana Thomson	Savills
Alice Kennedy	Savills

#### 3. Planning authority's views

Planning officers have attended pre-application meetings to discuss the development proposals for 191 – 201 Archway Road, and broadly support the proposals. The Quality Review panel's views on the architectural expression and quality of residential accommodation would be welcomed.

#### 4. Quality Review Panel's views

##### *Summary*

The panel broadly supports the development proposals for 191 – 201 Archway Road, which promise restoration of the existing 19<sup>th</sup> century façade, with high quality contemporary development behind. There remains scope for further refinement in the architectural expression of new elements of the scheme, including the rebuilt shop front. The density of development proposed also creates challenges in achieving high quality residential accommodation. The panel thinks that introduction of workspace could help address this, as well as adding to the vitality of the area. More detailed comments are provided below on: the commercial unit; Archway Road block; courtyard block; and mix of uses.

##### *Commercial unit*

- The panel understands that the existing Richardson of Highgate shop front is felt by local residents to make a positive contribution to the Highgate Conservation Area.
- In the panel's view, this shop front is not of particularly high quality, and breaks the rhythm of neighbouring buildings along Archway Road, which is characterised by smaller retail units with residential above and terraced houses.
- The panel supports the decision to rebuild the shop front, and would encourage the design team to consider how the rhythm of bays in the original 19<sup>th</sup> century building could be reflected more strongly in the façade at street level.

Report of Chair's Review Meeting  
20 May 2015  
QRP07 \_191 – 201 Archway Road



## CONFIDENTIAL

- A supermarket will occupy the commercial unit, and their commitment to bespoke signage for this site, sympathetic with the existing building, is welcome.
- The panel would also encourage planning officers to ensure through use of planning conditions that the fully glazed shop front is not blanked out by the supermarket operator.

### *Archway Road block*

- The panel supports the approach to the design of new residential accommodation behind the façade of the 19<sup>th</sup> century original building on Archway Road.
- The scheme proposes repair of the historic façade, with new windows, and contemporary architecture behind.
- The panel agrees that at first floor level, new high quality contemporary bay windows could be successful, but notes that repairs to the brickwork may be needed when the existing bay windows are removed.
- At fourth floor level, however, the panel would encourage careful restoration of the windows to their original proportions and detailing. Finding archive photographs of the original building would be helpful in this process.
- The new building behind the retained façade has been designed in brick and red zinc. The panel think this would benefit from refinement - to ensure that the aspiration for this to appear as a contemporary addition to be achieved.
- The detailed design of the deck access to the flats also needs further thought, to ensure an appropriate level of privacy to bedrooms.

### *Courtyard block*

- The courtyard block is the most challenging element of the scheme, and further work is needed to make a case for the density currently proposed.
- In terms of quality of life, the panel is concerned that residential units at ground and lower ground floor level will be compromised by lack of light.
- Considering alternative uses for this accommodation – such as workspace, or live / work units, could help resolve this.



## CONFIDENTIAL

- In terms of the building form and massing, the panel think that roof pitches running parallel to Causton Road could be more successful than the current proposal to run them at right angles with gables fronting Causton Road.
- This would simplify the junction between these roofs and the Archway Road block. It would also better continue the roof line of neighbouring houses on Causton Road.

### *Mix of use*

- The site currently accommodates both residential and workshop uses, and retaining some workspace in addition to the supermarket and gym would be welcome.
- For example, this could be a more appropriate use for accommodation at ground and lower ground floor level in the courtyard block, where limited daylight could result in poor quality homes.

### *Next steps*

The panel is confident that the project team will be able to address the points above, in consultation with Haringey officers.

